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### **TBPOC CONFERENCE CALL February 6, 2007**

| <b>AGENDA<br/>ITEM</b> | <b>DESCRIPTION</b>  |
|------------------------|---|
| <b>1</b>               | <b>2007 LEGISLATIVE UPDATE MEETING MATERIALS</b> <ul style="list-style-type: none"><li>a. 2007 Legislative Update (PowerPoint Presentation)*</li><li>b. 2007 Legislative Update (Report)*</li></ul>                     |
| <b>2</b>               | <b>Draft Fourth Quarter Report Ending December 31, 2006</b> <ul style="list-style-type: none"><li>a. 4<sup>th</sup> Quarter 2006 Report Schedule*</li><li>b. Fourth Quarter Report Ending December 31, 2006**</li></ul> |

\*Attachments

\*\*Stand-alone document e-mailed 2/2/07

## *Memorandum*

**TO:** Toll Bridge Program Oversight Committee (TBPOC)      **DATE:** February 2, 2006

**FR:** Program Management Team (PMT)

**RE:** Agenda No. - 1

Item- 2007 Legislative Update Meeting Materials

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**Cost:**

N/A

**Schedule Impacts:**

N/A

**Recommendation:**

Approval

**Discussion:**

The PMT has reviewed and requests approval of the attached draft materials and content for the TBPOC Legislative Update meeting scheduled for February 15, 2007 at the State Capitol.

**Attachments:**

- 1) 2007 Legislative Update (PowerPoint Presentation)
- 2) 2007 Legislative Update (Report)

# Will Kempton

Chairman

Toll Bridge Program Oversight Committee



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# Opening Comments



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## Toll Bridge Seismic Retrofit Program Report



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Fourth Quarter Report  
December 31, 2006

# 2006 4th Quarter Report



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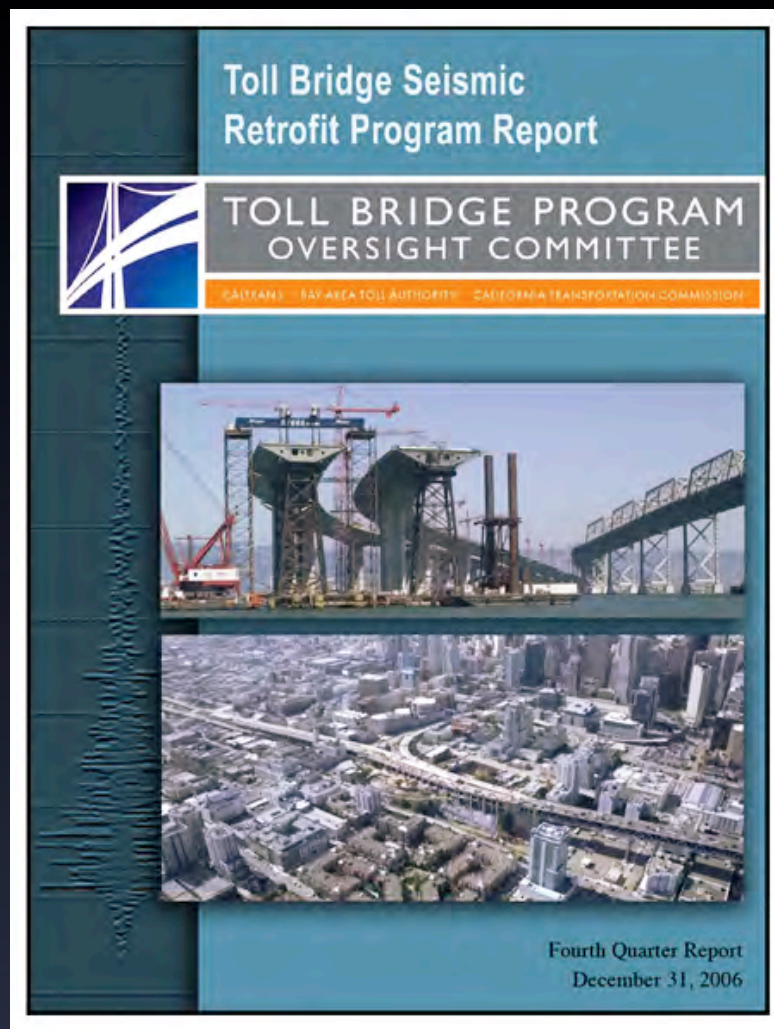
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## Costs and Schedule

Program is on budget and one year behind schedule

AB 144/SB 66 budget is \$8.685 billion

Current forecast is on budget: \$8.685 billion



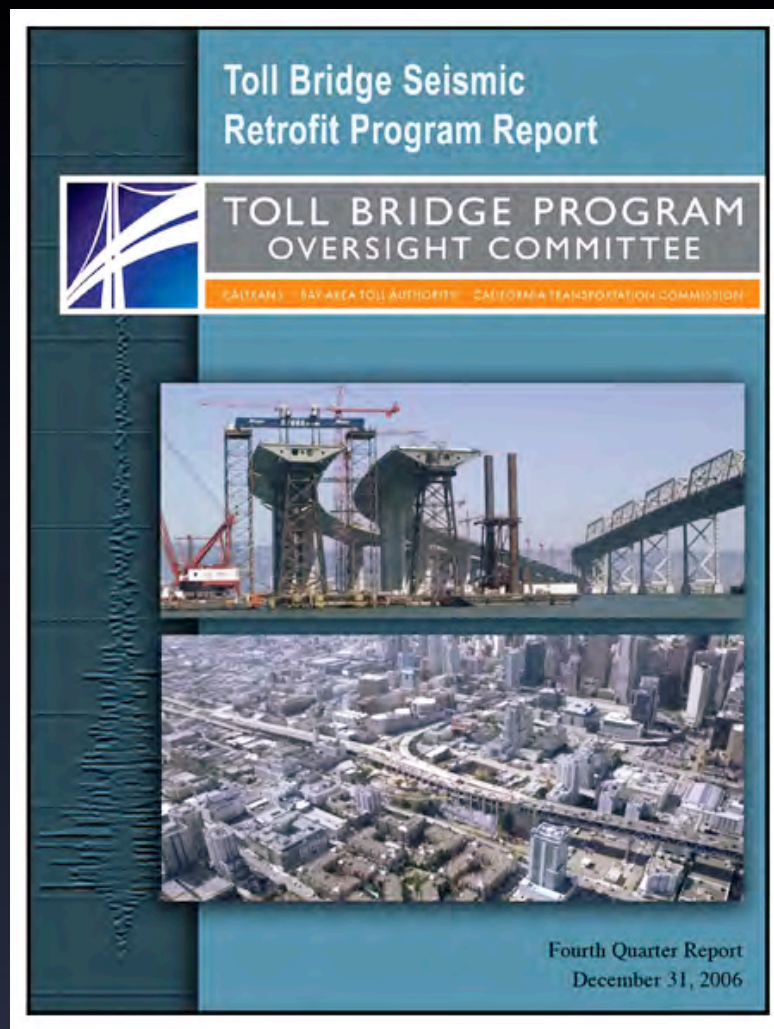
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## Costs and Schedule

Cost forecast for Bay Bridge East Span Replacement has increased \$48.3 million

Increase is more than offset by savings of \$89 million on Richmond-San Rafael Bridge Retrofit

Overall contingency reserve for Seismic Retrofit Program has increased to \$940.7 million from the \$900 million set by legislation



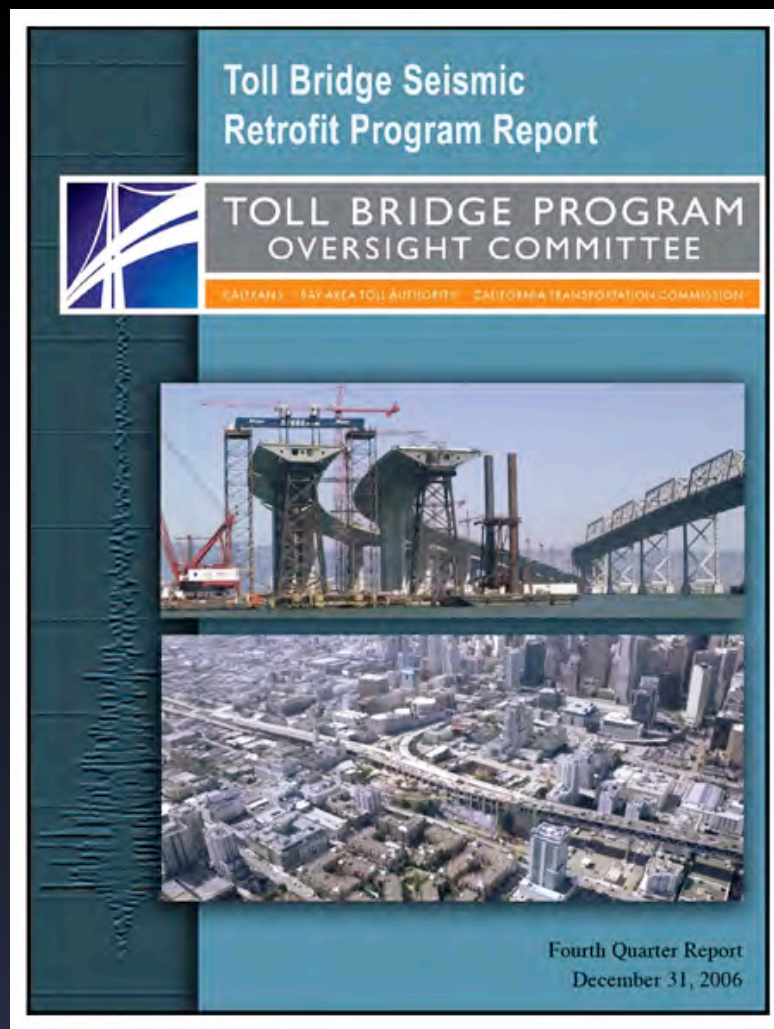
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## Costs and Schedule

AB 144/SB 66 target opening date East Span was Sept. 2011 for westbound and Sept. 2012 for eastbound.

Both dates have been extended by 12 months due to changes approved by the TBPOC in the SAS superstructure contract.



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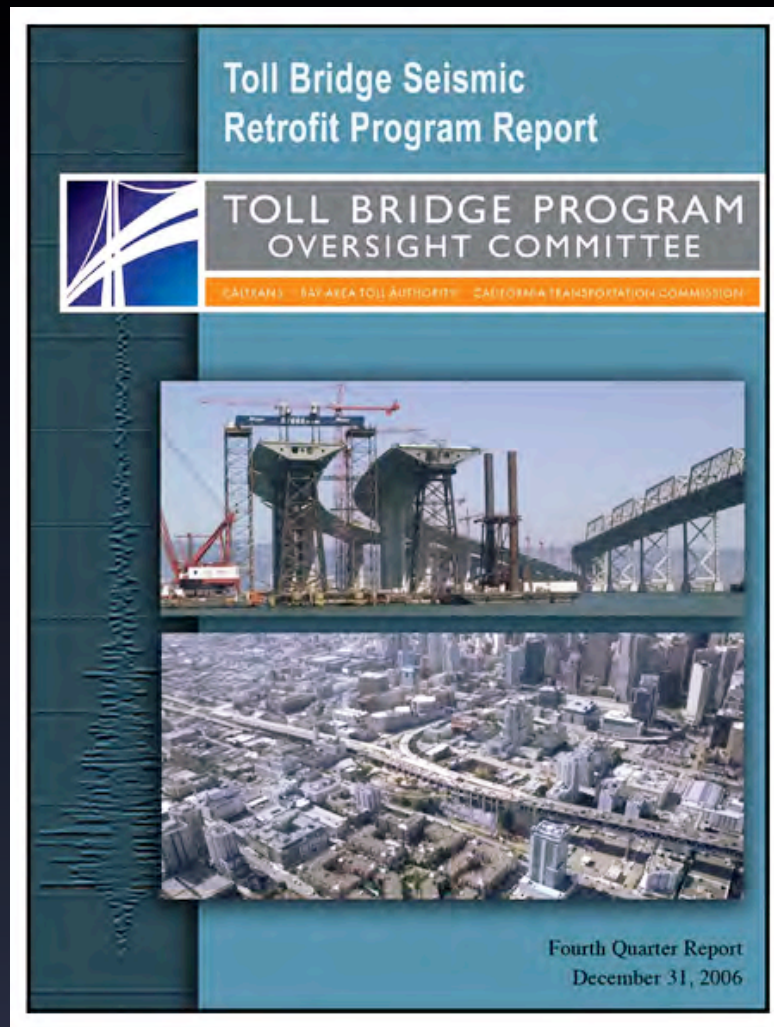
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## Costs and Schedule

Bay Bridge West Approach Replacement in San Francisco remains on budget at \$429 million

is on schedule for completion in August 2009.



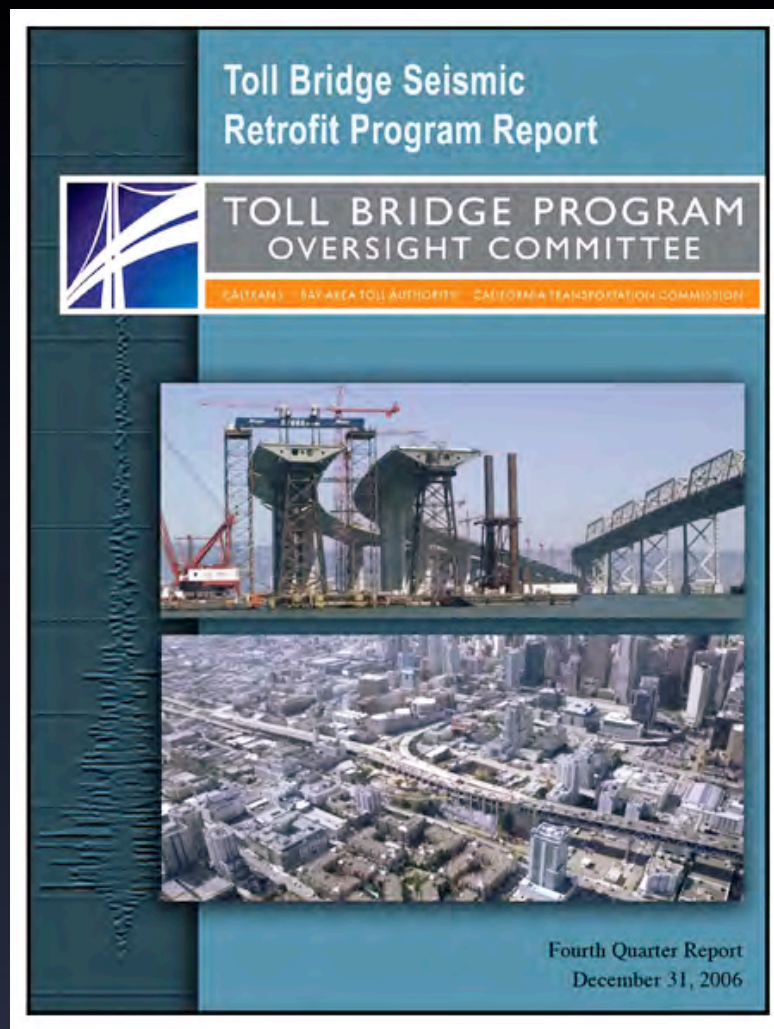
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## Costs and Schedule

All other Toll Bridge Seismic Retrofit Program projects are completed.



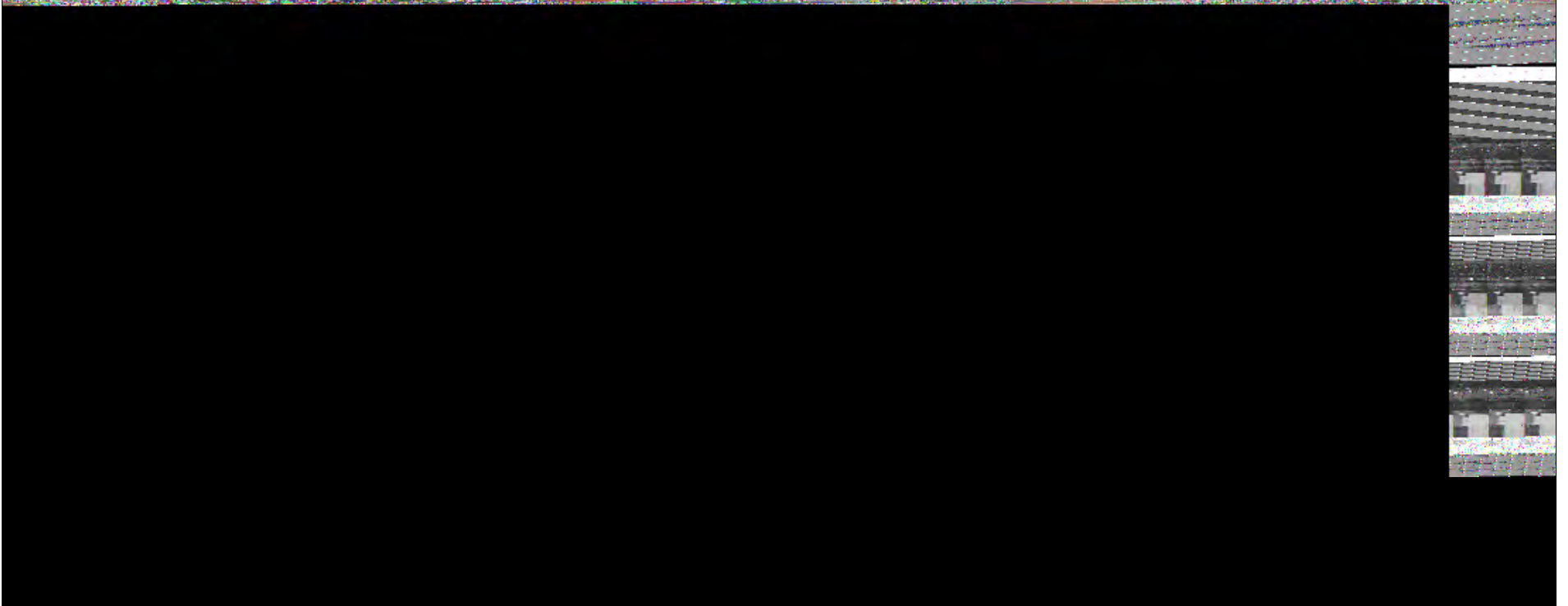
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# Major Challenges & Risk Issues

## Major Challenges 1

Public outreach on West Approach demolition, Labor Day Weekend closure of Bay Bridge and Benicia Bridge opening.

Continue finding way(s) to accelerate SAS project to ensure timely delivery and reduce project delivery costs





# Major Challenges & Risk Issues

Mitigating environmental impact of Bay Bridge East Span project:

- a) Bird and nest monitoring in active construction areas;

Major Challenges 2



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# Major Challenges & Risk Issues

Mitigating environmental  
impact of Bay Bridge East  
Span project:

- b) Monitoring of herring  
spawning activity  
continues through  
March 31

Major Challenges 3



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# Major Challenges & Risk Issues

Mitigating environmental impact of Bay Bridge East Span project:

- c) TBPOC is seeking approval to extend eelgrass pilot program

Major Challenges 4



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# Program Scope and History

**Tony Anziano**  
Toll Bridge Program Manager



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# Program Overview



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# Carquinez Bridge



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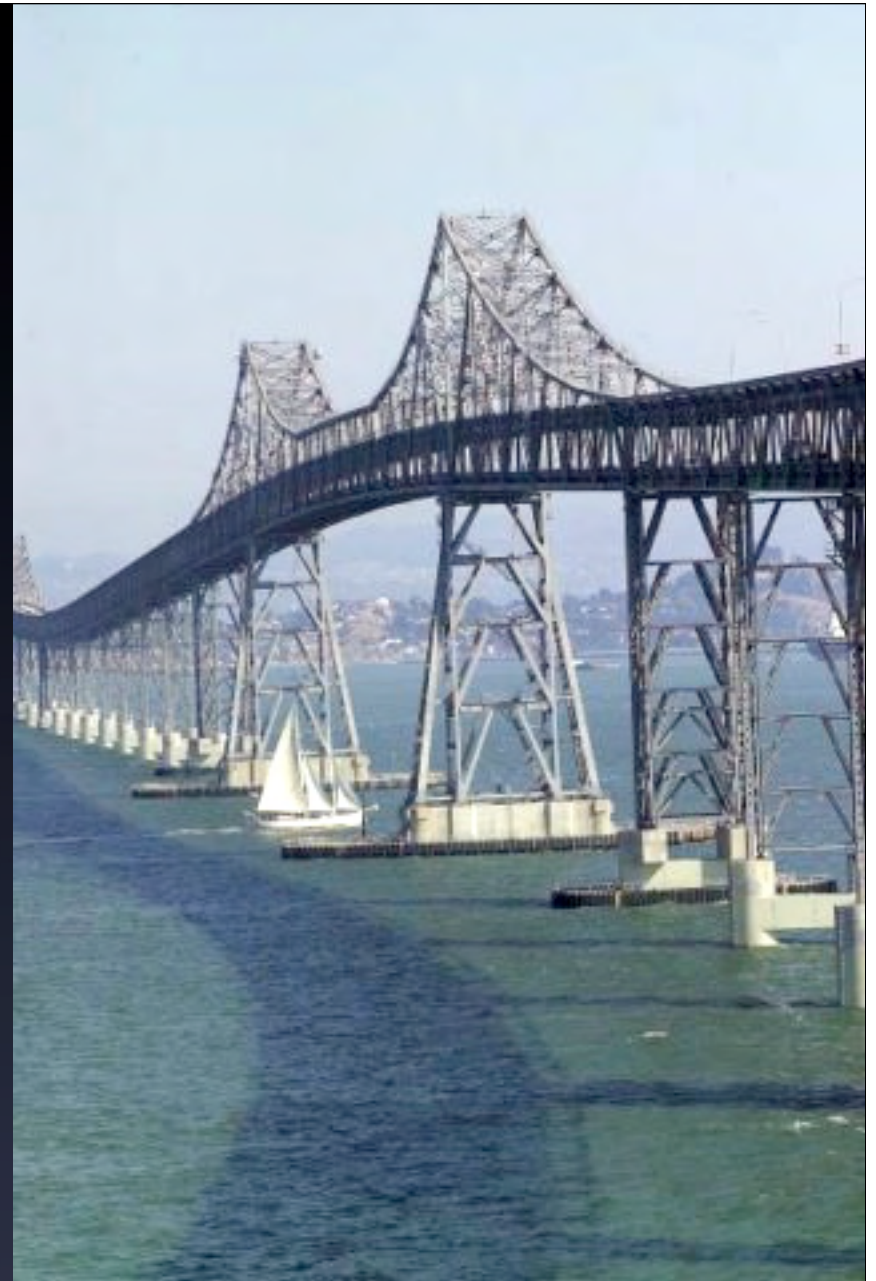
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Program Scope and History



# Richmond/San Rafael Bridge



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Program Scope and History

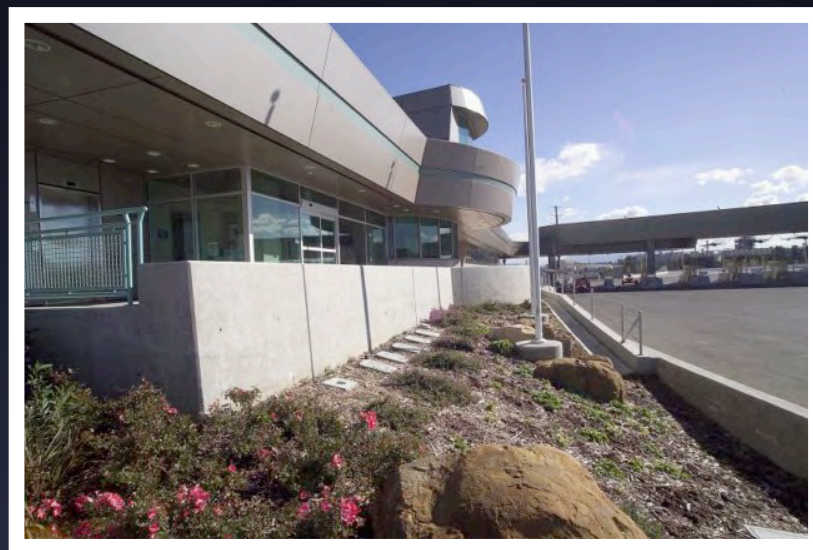
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# Benicia Bridge



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**Bart Ney**

Public Information Officer

San Francisco-Oakland Bay Bridge



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# Self Anchored Suspension Span Bid Awarded



## NEWS RELEASE

For Immediate Release

Contact: Bob Ney, Caltrans 916 224 6499  
Randy Rentschler, BATA 916 317 5780  
Stephen Miller, CTC 916 654 4245

*Toll Bridge Program Oversight Committee Works With Construction Industry:*

### New Bay Bridge SAS Bids Opened

Sacramento, Calif., March 22, 2006 – The Toll Bridge Program Oversight Committee (TBPOC) consisting of Caltrans, the Bay Area Toll Authority and the California Transportation Commission opened bids today for the Self Anchored Suspension (SAS) Bridge contract for the new Bay Bridge.

The apparent low bidder is American Bridge/Fluor Enterprises a Joint Venture who presented a bid for \$1.43 Billion. There were a total of 3 bids submitted, including a \$1.68 Billion bid by Kiewit/Koch Skanska/Morse a Joint Venture.

Caltrans Director Will Kempton said "This is great news for the Bay Area and the State of California. We can now move to get a safe bridge in place as quickly as possible."

Since the contract was advertised in August 2005, the TBPOC has worked closely with the construction industry to identify and implement key contract enhancements to the SAS in order to improve competitive bidding. Amendments included extending the bid advertisement period, extending the contract by one year, and enhancing incentives for contractor cost reduction. Three contractor outreach meetings were held, culminating in over 140 bidder inquiries.

The Caltrans engineer's estimate for the SAS is \$1.45 billion. The TBPOC has made efforts to reduce costs on the SAS contract, although construction and market factors influence the cost. Rising bonding and insurance costs, increased labor prices, and worldwide demand for construction equipment are factors. Shipping costs and the construction labor force have also been significantly impacted by Hurricanes Katrina and Rita.

"Today is a great day for our region. We're one step closer to seismic safety and a brand new bridge," said Steve Heminger, BATA Executive Director.

The review process will begin immediately, assessing the bids for responsiveness. The contract award is anticipated to occur in late April. The SAS is expected to be open to vehicle traffic in late 2013. Contractor incentives can potentially shorten the overall project construction up to six months.

"It is an historic day for transportation in California," commented John Barna, Executive Director for the California Transportation Commission.

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# Bay Bridge East Span Skyway Highlights



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# Transition Span Lift



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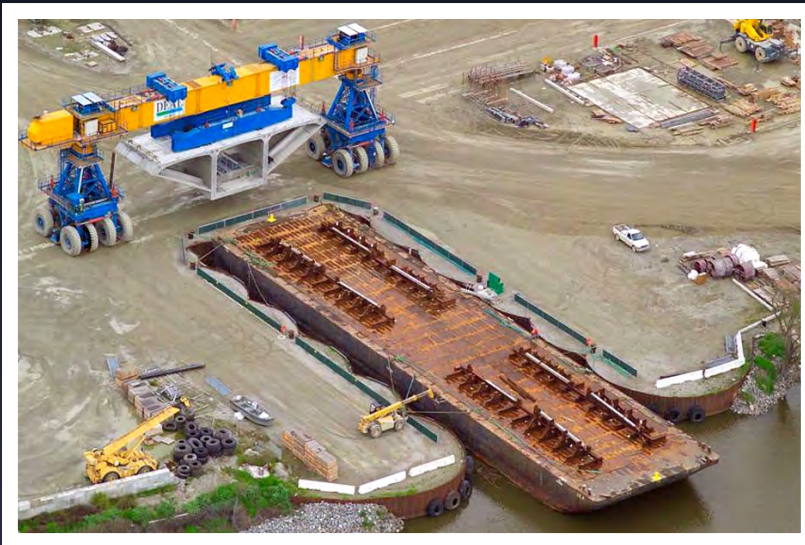
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# Stockton Yard Operations Completed



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# Final Skyway Segment Lift



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# West Approach Labor Day Demolition and Eastbound Bay Bridge Closure



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Movie goes here



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# West Approach Outreach Effort



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# West Approach Outreach Effort



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## ATTENTION Bay Area Motorists!

**The Bay Bridge Eastbound Will Be  
CLOSED Over Labor Day Weekend  
PLAN AHEAD!**

**BRIDGE TO BE CLOSED EASTBOUND**  
The lower (eastbound) deck of the Bay Bridge will be CLOSED from 11:59 PM Friday night, September 1 until 5:00 AM Tuesday morning, September 5. (Dates and times subject to change)


Expect traffic delays on nearby city streets and allow extra travel time.  
Motorists should use alternate bridges or take public transit.  
[CLICK HERE](#) for more information

**WHY LABOR DAY?**  
[CLICK HERE](#) to learn more.

**TAKE BART!**  
BART will run 24 hours at select stations  
Please visit [www.bart.gov](http://www.bart.gov) for schedule information

Thank you for your continued patience during this essential work.  
We're making your bridge safer!

**STAY INFORMED!**  
Visit [www.baybridgeinfo.org](http://www.baybridgeinfo.org) for more information.



THE SAN FRANCISCO-OAKLAND  
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We're making history.

Bay Bridge Corridor

WEST APPROACH WEST SPAN YR & T1 SAG EAST SPAN DEMO SKYWAY OAKLAND TOUCHDOWN

## BAY BRIDGE EASTBOUND WILL BE CLOSED

**Labor Day Weekend, September 1-5, 2006**  
Beginning 11:59pm Friday night, until 5:00am Tuesday morning.

**Avoid major traffic delays.  
Plan alternate routes or take transit.**

**LEARN MORE >>**

**Bay Bridge Eastbound (Lower Deck) will be closed Labor Day Weekend -View the Informational Factsheet**

**NEW!** Labor Day Video Public Service Announcement

**NEW!** Labor Day Construction Radio Announcement

[Labor Day Construction Over-View Map with Traffic Flow Alignment](#)

[For Transportation Options to The Art and Soul Festival in Oakland Click Here](#)

[Dial 5-1-1 or visit 511.org](#) for public transit options and traffic information over Labor Day weekend

**BART**  
[www.bart.gov](http://www.bart.gov)

**ACTransit**  
[www.actransit.org](http://www.actransit.org)

**San Francisco Municipal Railway**  
[www.sfmuni.com](http://www.sfmuni.com)

**Bay Bridge Program Oversight Committee**

Arnold Schwarzenegger  
Governor

Sumie Wright McPeak  
Secretary of Business, Transportation and Housing Agency

Marion Bergeson  
Chair, California Transportation Commission

Jon Rubin  
Chair, Bay Area Toll Authority

Will Kempton  
Director, Caltrans

John Bama  
Executive Director, California Transportation Commission

Steve Heminger  
Executive Director, Bay Area Toll Authority



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# West Approach Outreach Effort



## THE SAN FRANCISCO-OAKLAND BAY BRIDGE SEISMIC SAFETY PROJECTS

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### CLOCKTOWER RESIDENT UPDATE

JULY 20, 2006

Thank you for your continued patience and cooperation during the Bay Bridge Seismic Safety Retrofit Projects on the West Approach. We would not be able to accomplish this essential work without support from our neighbors!

Please note that Caltrans and its partners are continually striving to minimize disruptions to Clocktower by expediting the completion of work. We fully acknowledge, however, that despite our best efforts, demolition and construction work of the magnitude affects neighboring communities. We understand that nighttime work, especially, has affected Clocktower residents. We do our best to avoid scheduling work during night-time hours, but when major lane closures are required and traffic is heavily impacted, we must perform work at night. We sincerely regret any inconvenience this may cause.

Below is a brief Progress Report of our work in your area to date, and a description of upcoming work that will affect residents of Clocktower. We are pleased to inform you that almost 70% of the work has already been completed and anticipate that the work affecting Clocktower will be entirely done by late 2006. We will continue to provide regular updates prior to the beginning of major work that impacts your neighborhood.



Please refer to this color-coded diagram indicating the project areas while reviewing the work outline below.

#### PROGRESS REPORT:

Seismic retrofit work on the West Approach started in June, 2003. Over the past three years, a significant amount of work has been completed, as described below. The majority of the work affecting Clocktower residents has already been finished. We anticipate that the work having an effect on Clocktower will be completed by Summer 2006.

Below is a detailed description of work completed to date:

- The area shaded in red (Frame 7U North) has been completed, including the demolition, preparation for reconstruction (framework erection) and the reconstruction work.
- The demolition of the Harrison Street off-ramp has been completed. Please note that in order to minimize the impact of this work on the neighborhood, the demolition, which was originally slated to continue for three months, was accomplished in just one week and. We hope that by consolidating the work into one weekend, we reduced the inconvenience to Clocktower residents.
- The demolition of the area shaded in green (Frame 8U North) has been completed and the reconstruction of this portion is currently ongoing.



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# West Approach Outreach Effort

**THE SAN FRANCISCO-OAKLAND  
BAY BRIDGE SEISMIC SAFETY PROJECTS**

**Bay Bridge Eastbound CLOSED  
Labor Day Weekend**

**CLOSURES ARE IN THE EASTBOUND DIRECTION ONLY.  
The Bay Bridge westbound into San Francisco will remain open throughout the weekend.**

**ATTENTION MOTORISTS!**  
The Bay Bridge eastbound deck and several westbound on-ramps (Ramp 5, Ramp 6, Ramp 7, Ramp 8, Ramp 9) will be closed from 11:59 pm Friday night, September 1, until 5:00 am Tuesday morning, September 5. Dates and times subject to change.

**PLEASE PLAN AHEAD:**

- Allow extra time in your travel plans.
- Use public transit, including BART, buses, and ferries.
- Use alternate bridges.

**PLEASE NOTE:**  
Milepost 1 leaving town for the Labor Day weekend will be able to cross the bridge westbound until 11:59 pm Friday evening. Before the lower deck is closed.

The eastbound closure is scheduled to occur after the 40th pre-season game scheduled for Friday evening.

**TRANSIT ALTERNATIVES:**  
BART will be running overnight service to select stations\* Friday night through 1:00 a.m. Tuesday morning. Regular BART service resumes at 4:00 a.m. Tuesday, Sept. 5. Go to [www.bart.gov](http://www.bart.gov) for schedule information.

**AC Transit:** go to [www.actransit.org](http://www.actransit.org) for modified schedules.

**SEMI:** Line 108 will be running a modified schedule. Go to [www.semita.com](http://www.semita.com).

**Alameda-Rak Ranch Ferry and Vallejo-Bay Bridge Ferry:** will be running expanded service. Go to [www.eastbayferry.com](http://www.eastbayferry.com) and [www.baybridgeferry.com](http://www.baybridgeferry.com).

**BART STATIONS OFFERING EXTENDED SERVICE:**

|                  |               |
|------------------|---------------|
| Bay Fair         | Embarcadero   |
| East Bay         | Marina        |
| Colma            | San Francisco |
| Castroville      | San Francisco |
| Concord          | Walrus Creek  |
| Daly City        | 72nd St. Ave  |
| Dublin           | Jack London   |
| El Cerrito Norte |               |

**SOME PEOPLE HAVE ASKED:**  
"WHY LABOR DAY WEEKEND?"

- We will be demolishing more than 1,000 feet of the upper deck from Base Street west to the Spittoburn Building. Seismic public safety: the lower deck must be closed during this work.
- By working over Labor Day weekend, we can complete this phase in three days and avoid a second weekend closure.
- Saturday and Sunday traffic volumes on Labor Day weekend are typically lower than on other weekends. Traffic volumes on Labor Day itself are significantly lower than on a normal weekend or weekday.
- Weekends in September and October have major public events planned. By performing this work over Labor Day weekend, we eliminate the possibility of greater traffic and community impacts.
- Work must be done between mid-August and the end of October to keep this important seismic safety project on schedule.

Visit [www.S17.org](http://www.S17.org) or Call 511 for transit planning.

Eastbound deck closed from 11:59 pm Fri., Sep. 1 until 5 am Tue., Sep. 5

Stay Informed! Updated closure information available at [www.baybridgeinfo.org](http://www.baybridgeinfo.org)

CONTACT US  
Bay Bridge West Approach  
Public Information Office  
415 Mission St.  
San Francisco, CA 94105  
Toll-free: 1-877-347-5882

Use Alternate Bridges:

- Golden Gate Bridge
- Richmond-San Rafael Bridge
- San Mateo-Hayward Bridge

Thank you for your continued cooperation during this essential work.

**NTPAW 2006**

**AASHTO Public Affairs  
Skills Award**

Print  
Media Kit  
Presented to

**California Department  
of Transportation (Caltrans)**

Aug. 1, 2006



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# West Approach Multi-Agency Effort

AC Transit

Alameda/Oakland Ferry

Amtrak

BART

California Highway Patrol

City and County of San Francisco

Golden Gate Transit

Greyhound

Metropolitan Transportation Commission

MUNI

Samtrans

Vallejo Ferry



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# West Approach Transit Coordination



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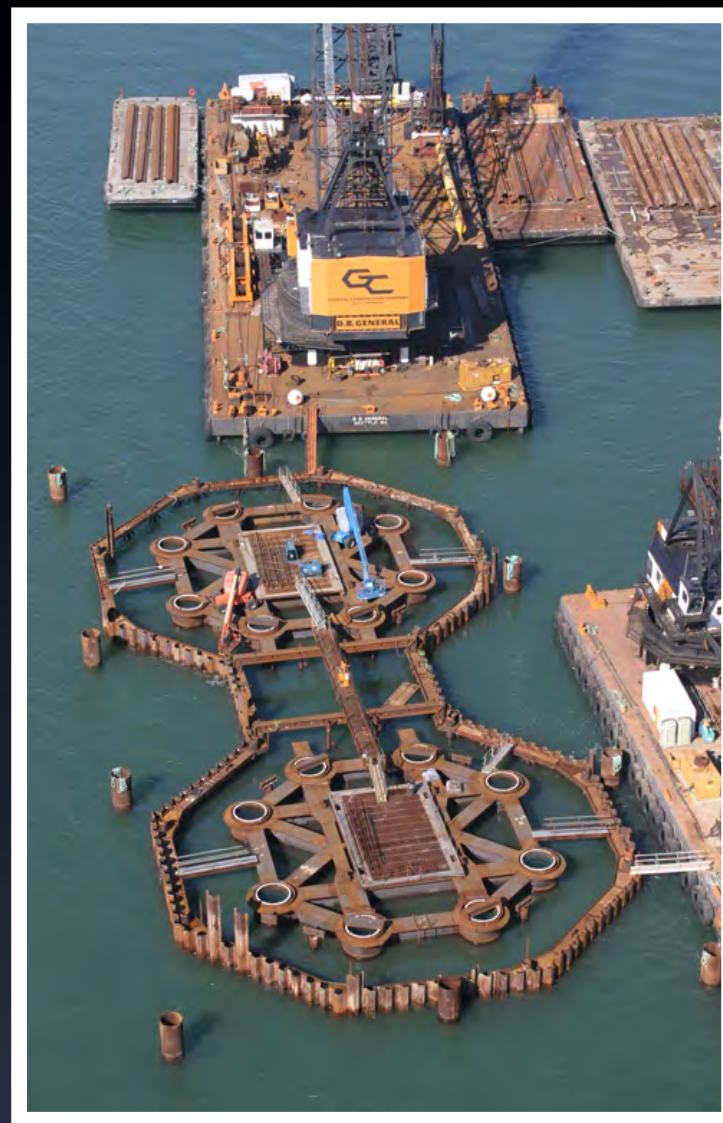
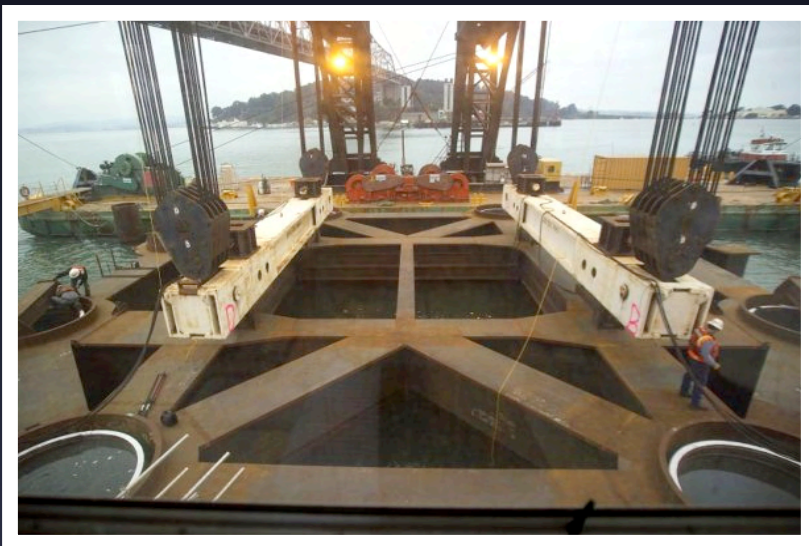
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# E2-T1 Completion



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# West Approach Temporary Bypass Eastbound



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2007 Look Ahead



# East Span West Tie-In Three Day Closure



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Skyway Contract  
will be completed!



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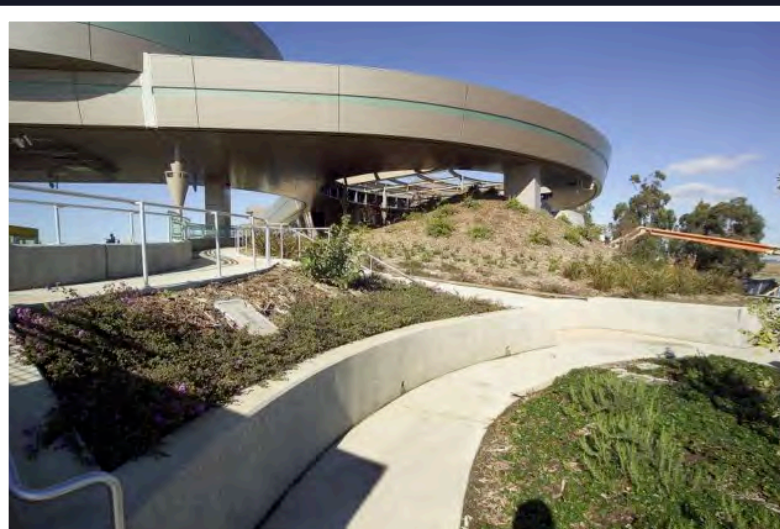
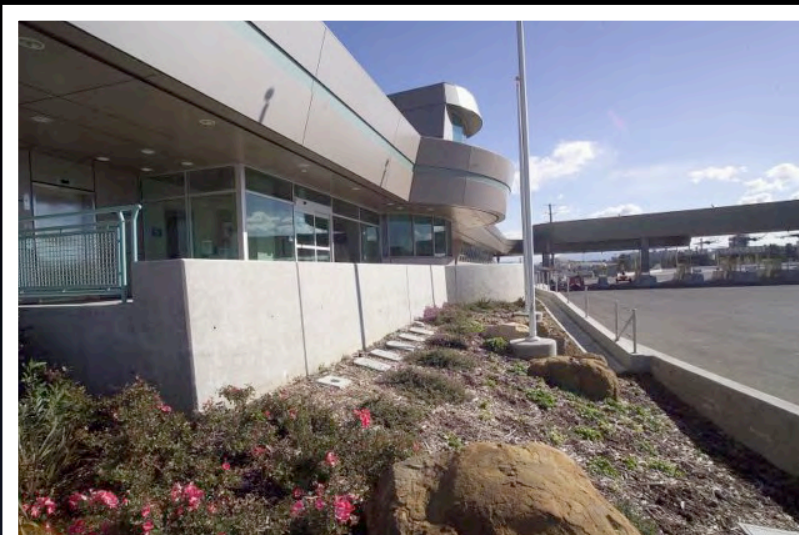
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# Benicia Bridge Opening Event



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# Closing Remarks



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# Steve Heminger

Executive Director

Metropolitan Transportation Commission

Toll Bridge Program Oversight Committee Member



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John Barna

Executive Director

California Transportation Commission

Toll Bridge Program Oversight Committee Member



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# Will Kempton

Director

California Department of Transportation

Toll Bridge Program Oversight Committee Chairman



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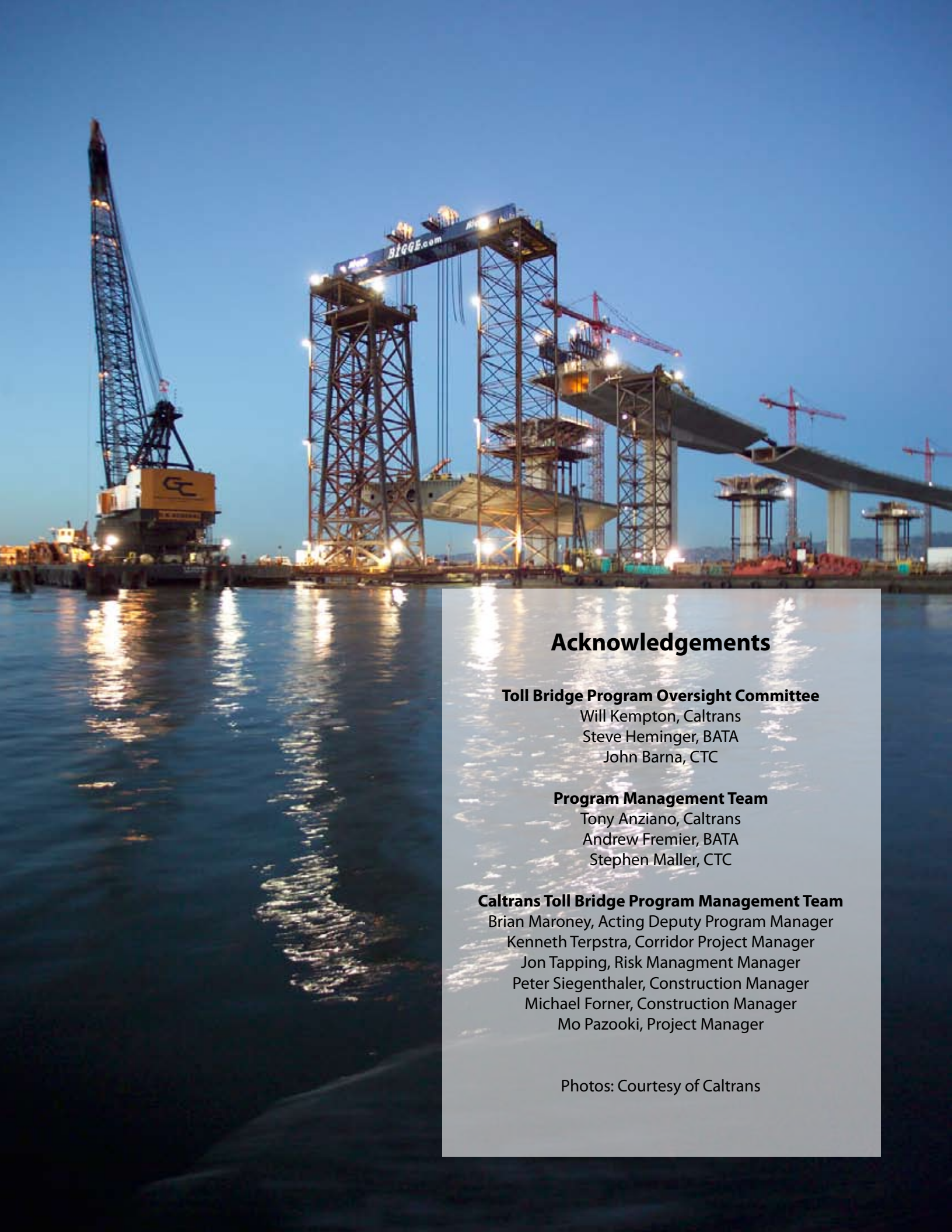
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## 2007 Legislative Update - DRAFT

February 2007







## **Acknowledgements**

### **Toll Bridge Program Oversight Committee**

Will Kempton, Caltrans  
Steve Heminger, BATA  
John Barna, CTC

### **Program Management Team**

Tony Anziano, Caltrans  
Andrew Fremier, BATA  
Stephen Maller, CTC

### **Caltrans Toll Bridge Program Management Team**

Brian Maroney, Acting Deputy Program Manager  
Kenneth Terpstra, Corridor Project Manager  
Jon Tapping, Risk Management Manager  
Peter Siegenthaler, Construction Manager  
Michael Forner, Construction Manager  
Mo Pazooki, Project Manager

Photos: Courtesy of Caltrans

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# Executive Summary





## Executive Summary

To Members of the Legislature,

With the passage of Assembly Bill 144 in July 2005, the State Legislature presented a road map for completing essential seismic safety work on state-owned toll bridges. One of the key requirements of this visionary legislation was the establishment of the Toll Bridge Program Oversight Committee (TBPOC) – an interagency partnership between the California Department of Transportation (Caltrans), the California Transportation Commission (CTC), and the Bay Area Toll Authority (BATA). Our mission is to ensure that the seismic safety work – the most ambitious retrofit program on record – is completed in a cooperative, expeditious, and cost effective manner. Included within the purview of the TBPOC are the completion of the new East Span of the San Francisco-Oakland Bay Bridge and the new Benicia-Martinez Bridge, which is funded through Regional Measure 1 (RM1).

In 2006 we focused on the multiple and highly complex “mega projects” on the Bay Bridge. In addition to moving forward with projects on the bridge’s East Span, we made significant progress with the retrofit-by-replacement of the one-mile I-80 West Approach to the bridge in San Francisco. Our agencies are working cooperatively at all levels – from design and construction, to scheduling and fiscal risk management – to ensure our continued success.

Last year marked the awarding of the largest public works contract in state history; the successful completion of two of the heaviest lifts on record; the orchestration of complex demolition and construction work; and equally complex traffic shifts. Much of this work required extensive public outreach. Key to our success in the past year was the assistance that we received from state legislators and other stakeholders in garnering public support. As a result, we have made major inroads in informing your constituents about the importance of the Bay Bridge Seismic Safety Projects.

This Legislative Update highlights some of our most significant accomplishments in 2006 and looks ahead at the challenges that we anticipate in 2007.

### Highlights of 2006

#### **San Francisco-Oakland Bay Bridge:**

- The enactment of AB 144 enabled us to resume work in late 2005 on the marine foundation and tower pier (E2-T1) for the new Self-Anchored Suspension (SAS) span, which will be the signature span of the new bridge. We were also able to move forward with the competitive bid process to build the SAS – awarding a contract to American Bridge/Fluor Enterprises (ABF), a Joint Venture, as the low bidder, with a bid below the project estimate.
- In August 2006, a major milestone for the 1.2-mile Skyway was reached with the successful lift of a 1,750-ton steel segment, one of two enormous steel segments that will connect the Skyway to the SAS.
- In December 2006, the last of 452 pre-cast concrete segments comprising the Skyway was hoisted, completing the bridge structure.
- Major milestones were also reached in 2006 on the one-mile stretch of freeway leading to the bridge from San Francisco, known as the West Approach. The elaborately staged demolition and construction work required the most intensive effort to date in public outreach, transit coordination, and interagency cooperation – most notably over Labor Day weekend, when the entire lower deck of the bridge was closed to traffic for nearly 77 hours.

#### **Richmond-San Rafael Bridge:**

- The Richmond-San Rafael Bridge seismic retrofit project was completed in [month, 2005], \$89 million under budget. In October of 2006, our Committee authorized the transfer of these cost savings to the Toll Bridge Seismic Retrofit Program’s Contingency, which is currently forecast at \$940.7 million.



**Benicia-Martinez Bridge:**

- Spanning the Carquinez Strait and linking Contra Costa and Solano Counties, the new Benicia-Martinez Bridge neared completion in 2006. Funded through Regional Measure 1 at a cost of \$1.3 billion, the new bridge will carry five lanes of northbound traffic and will open in December, 2007.

**2007 Look Ahead**

The coming year presents numerous complex construction activities, as well as the anticipated completion of several significant projects. These projects, some representing firsts in design and construction, will present significant challenges next year and through completion. Our risk management teams will continue to coordinate corridor schedules and project delivery to help ensure our success.

Most notably, we will continue to work towards completing the series of highly complex projects on the West Approach and East Span of the Bay Bridge. The major activities planned for 2007 include:

- The final major traffic shift from the West Approach onto a temporary structure will occur, followed by the demolition and reconstruction of the eastbound viaduct;
- The final touches will be completed on the Skyway section of the bridge, with a celebration ceremony slated for year's end;
- Completion of the marine foundations for the Self-Anchored Suspension span, which will be the first in a series of four contracts to build the Oakland touchdown structure;
- Finally, and most significantly, the first phase of West Tie-In near Yerba Buena Island will be completed in 2007, requiring a full bridge closure over a three-day weekend. The closure will be modeled after the successful public outreach and transit agency coordination executed for the Labor Day Weekend 2006 closure. The 2007 weekend closure will help expedite the construction schedule and reduce the amount of time motorists will need to use the temporary bypass structure that is being built to enable the construction of the SAS.

Lastly, the year 2007 will also mark the opening of the new Benicia-Martinez Bridge.

As we progress, we will continue to keep you and your constituents informed. Your continued support will remain essential to our success. Thank you.

**Toll Bridge Program Oversight Committee**

Will Kempton, Chair  
Director, Caltrans

John Barna  
Executive Director  
California Transportation Commission

Steve Heminger  
Executive Director  
Bay Area Toll Authority





# **Toll Bridge Program Overview**





The year 2006 was a landmark year for the Toll Bridge Program, marking nearly 10 years of operation. In 1997, Senate Bills (SB) 60 and 226 were signed into law, establishing the Toll Bridge Seismic Retrofit Program, which gave the California Department of Transportation (Caltrans) the responsibility and authority to retrofit six of the seven State-owned toll bridges (see Table 1) in order to strengthen, preserve and maintain California's renowned highway and bridge network. Under the statute, the Metropolitan Transportation Commission (MTC), through the affiliated Bay Area Toll Authority (BATA) was assigned the responsibility for selecting the final design for the East Span of the San Francisco-Oakland Bay Bridge, and Caltrans was assigned the responsibility of the design and construction of the new bridge.

While the Toll Bridge Program was established 10 years ago, the seismic retrofit of San Francisco Bay Area bridges began almost 20 years ago when Bay Area voters approved Regional Measure 1 (RM1), authorizing an increase in Bay Area bridge tolls. The funding from RM1 was earmarked for toll bridge con-

gestion relief projects, including the construction of the new Benicia-Martinez Bridge. The opening of the Benicia-Martinez Bridge, the completion of the Richmond-San Rafael Bridge Public Access Project, and significant milestones

on the Bay Bridge project will comprise the major highlights of the Toll Bridge Program in 2007. The projects that are included in the Toll Bridge Program are shown in Table 1.



Bay Bridge East Span, Loma Prieta Earthquake 1989

# Toll Bridge Program Overview

Table 1: Toll Bridge Seismic Retrofit Program

|   |   |              |
|---|---|--------------|
| 1 | San Francisco-Oakland Bay Bridge East Span Replacement      | Construction |
|   | San Francisco-Oakland Bay Bridge West Approach Replacement  | Construction |
|   | San Francisco-Oakland Bay Bridge West Span Seismic Retrofit | Complete     |
| 2 | San Mateo-Hayward Bridge Seismic Retrofit                   | Complete     |
| 3 | Richmond-San Rafael Bridge Seismic Retrofit                 | Complete     |
| 4 | Eastbound Carquinez Bridge Seismic Retrofit                 | Complete     |
| 5 | Benicia-Martinez Bridge Seismic Retrofit                    | Complete     |
|   | New Benicia Martinez Bridge (Regional Measure 1)            | Construction |
| 6 | San Diego-Coronado Bridge Seismic Retrofit                  | Complete     |
| 7 | Vincent Thomas Bridge Seismic Retrofit                      | Complete     |

Source: Toll Bridge Seismic Retrofit and Regional Measure 1 Programs, Monthly Progress Report December 2006, Toll Bridge Program Oversight Committee.



## TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

In July 2005, the passage of Assembly Bill (AB) 144 combined the remaining elements of Regional Measure 1 with the Toll Bridge Seismic Retrofit Program and created the Toll Bridge Program Oversight Committee (TBPOC) to implement a project oversight and project control process for the program. The TBPOC consists of the directors of the three partnering agencies: Caltrans, BATA, and the California Transportation Commission (CTC).

*The legislative mandate of the TBPOC is as follows (according to AB 144, Chapter 71, Section 10c):*

*The Toll Bridge Program Oversight Committee, created pursuant to Section 30952.1, shall implement a project oversight and project control process for the Benicia-Martinez Bridge project and the state toll bridge seismic retrofit program projects. The committee's project oversight and control processes shall include, but not be limited to, reviewing bid specifications and documents, providing field staff to review ongoing costs, reviewing and approving significant change orders and claims, and preparing project reports.*

*Furthermore, as defined by Section 30952.1, the TBPOC does the following:*

- Review project status, program costs, and schedules;
- Resolve project issues;
- Evaluate project changes;
- Develop and regularly update cost estimates, risk assessments, and cashflow requirements for all phases of the toll bridge projects; and,
- Provide program direction.

The year 2006 represented the first full year of the Committee's operation. The TBPOC has reviewed, provided recommendations, and approved contractual, budgetary and schedule related issues in a timely fashion with the support from member agencies. The

TBPOC authorized splitting the Yerba Buena Transition Structure and Oakland Touchdown projects in order to facilitate construction efficiently. The TBPOC continues to identify, manage, and address schedule and cost related risks.

Supporting the TPPOC is the Program Management Team (PMT), which consists of management staff from the three partnering agencies. The PMT meets on a regular basis, further facilitating interagency coordination and progress.

## PROJECT TEAMS

There are several hundred individuals involved in designing, constructing, scheduling, and managing the Seismic Safety Projects. They are organized into a sophisticated network of interdisciplinary teams, with the objective of "project delivery" – the design and construction of each of the many projects that are a part of the Seismic Safety Program. Each project has teams of engineers, contractors, and support staff who collaborate in the areas of Design & Construction, Corridor Scheduling, Risk Management, Safety, and Maintenance to successfully deliver each project. The project delivery process is illustrated in Figure 1.

From design through construction, there are multiple phases of review and oversight, during which the various project delivery teams are responsible for the evaluation of potential risks, the coordination of project schedules, on-site inspection and safety, and maintenance, to name a few. Construction teams may include representatives from Bay Bridge Public Information, as well as representatives from local and regional agencies, including transportation agencies, and emergency services, who work together on major construction projects involving the public.

## RISK MANAGEMENT

Assembly Bill 144 requires Caltrans to develop and implement an expanded, comprehensive risk management plan

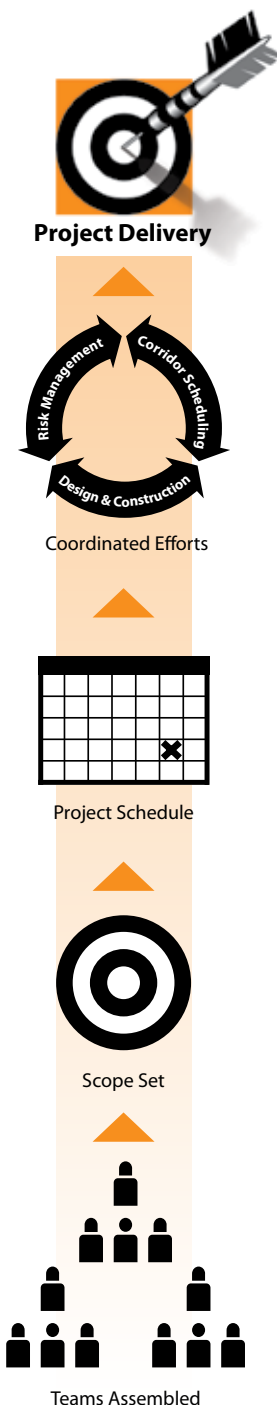


Figure 1: Project Delivery

for the Toll Bridge Program to augment the established risk management protocols and mitigation measures already in place. The Toll Bridge Program includes the largest Caltrans project to date, presenting a variety of engineering and construction challenges. In response, Caltrans has developed a comprehensive risk management plan,



which includes state-of-the-art methods, tools and processes for managing and minimizing risk by:

- Obtaining early warning of challenges to program goals and targets, and of opportunities for improving prospects;
- Providing the Toll Bridge Program management with focused information to support budget and schedule forecasting, effective risk-taking, and on-going program and project corrections;
- Reducing the potential for cost and schedule overruns;
- Establishing and maintaining adequate contingency reserves.

The substantial evolution of the Toll Bridge Risk Management Program is evidenced by the appointment of the Risk Management Coordinator to a Transportation Research Board committee that develops guidance on risk management of transportation projects nationwide. In addition, the Toll Bridge Risk Management Team (RMT) has been asked to assist other Caltrans districts with developing their risk management capabilities. Caltrans has assembled a Risk Management Team (RMT) for each of the contracts and shares risk management information with the TBPOC. With the continuing support and cooperation of project teams and the agencies, risk management has become an integral compo-

nent of program and project management. In a continuous process (Figure 2) of which communication is a critical component, the RMT works with the Design and Construction teams to continually monitor, minimize, or eliminate risks. The RMT has developed an effective approach which focuses on managing the most significant risks, typically identified by having both a high probability of occurrence and high impact on project delivery.

Notable achievements in 2006 include:

- The receipt of multiple competitive bids on the SAS project as a result of innovative quantitative cost and schedule risk analyses.
- The active management of risks by assigning to each contract a cross-functional risk response team who worked closely with representatives from partnering agencies;
- Integrated and coordinated contract schedules developed by the Schedule team, who identified potential risks and evaluated opportunities to shorten overall construction duration;
- Facilitation of budget and schedule forecasting through quantitative risk analysis input provided to the program management team.

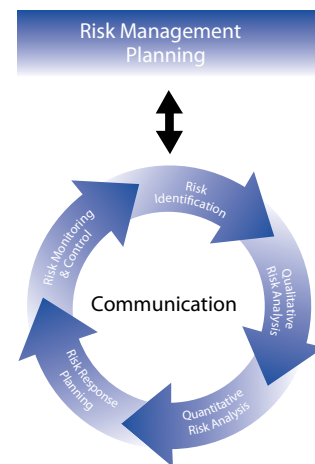
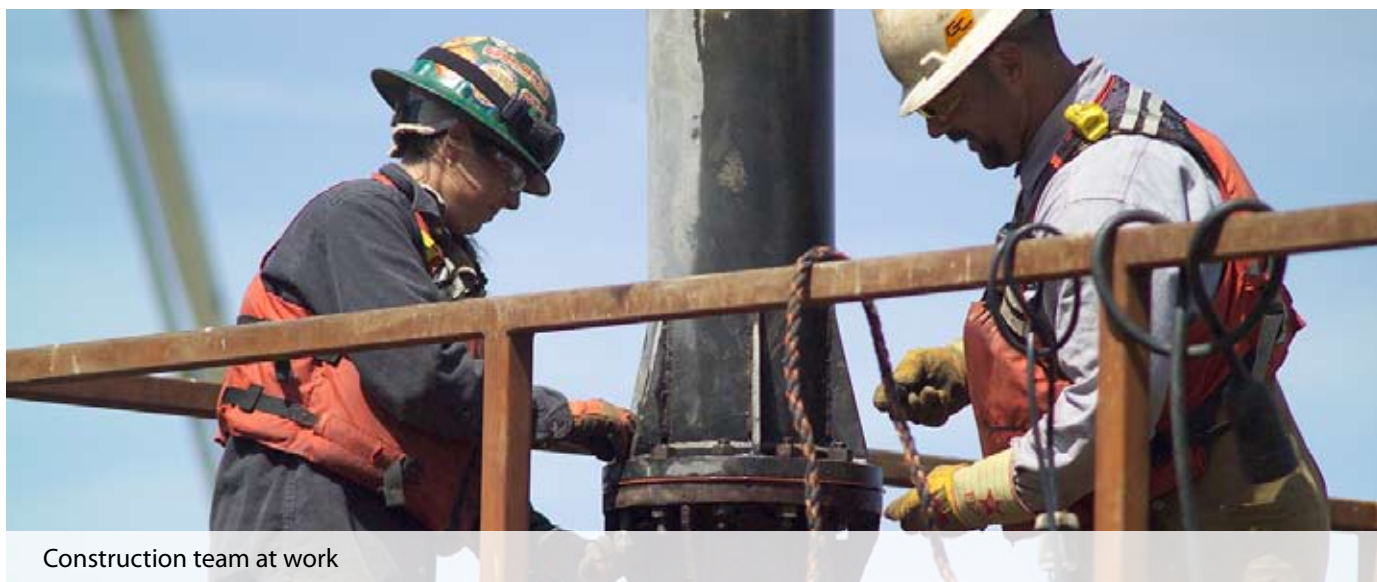


Figure 2: Risk Management

## PROGRAM FUNDING AND BUDGET

The AB 144/SB 66 baseline budget for the Toll Bridge Program is \$8.7 billion (see Appendix A, Table 1). The ongoing projects will be constructed within the approved baseline budget for the Toll Bridge Program. See Appendix A, Table 2 for the approved budget for each contract.

BATA has direct programmatic responsibilities. The Bay Bridge project has been funded based on a BATA approved Finance Plan, dependent on toll revenues from the state-owned bridges. BATA has authorized a toll increase on all state-owned Bay Area bridges by \$1.00 (from \$3.00 to \$4.00) effective as of January 1, 2007.



Construction team at work





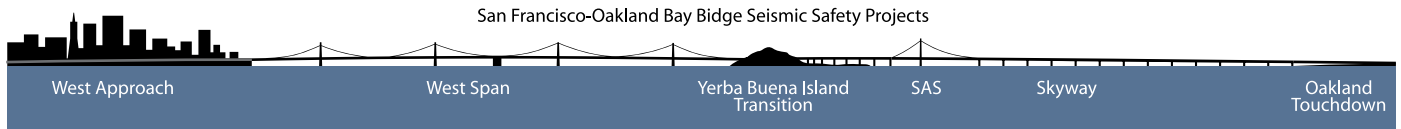
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# San Francisco - Oakland Bay Bridge







Carrying an average of 280,000 vehicles a day, the Bay Bridge is the third busiest bridge in the nation. The existing bridge consists of three major components: West Approach, West Span, and East Span. The series of Bay Bridge Seismic Safety Projects currently underway represents the most ambitious public works undertaking in California's history. Keeping the large volume of traffic flowing as major construction work progresses has been a considerable challenge during each of these projects and has required innovative scheduling, staging, and traffic realignments. Public outreach efforts have greatly assisted in gaining public understanding and support for the Bay Bridge construction activities, and the patience and cooperation of motorists have enabled the accomplishment of what at times seemed impossible

# San Francisco - Oakland Bay Bridge

## 2006 Highlights

- *SAS bid award*
- *West Approach Labor Day  
Public Outreach  
Public Transit Coordination  
Multi-Agency Effort*
- *Skyway Highlights  
Transition Span  
Stockton Yard Completion  
Segment Erection*

– a full lower deck closure for nearly 77 hours.

Projects of this magnitude must include a wide array of stakeholders, including legislators, transportation agencies, local, regional and federal government, community and special interest groups, motorists, the media, and the general public. The work has involved significant outreach on the ground level through meetings, canvassing, and one-on-one outreach at the neighborhood level, most notably near the



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Self-Anchored Suspension Span and Skyway

West Approach in San Francisco. It has also required extensive environmental review, monitoring, and mitigation in and near the San Francisco Bay in environmentally sensitive marine habitat.

### One Bridge: Many Mega Projects

Following the 1989 Loma Prieta Earthquake, when a 250-ton section of the East Span's upper deck collapsed, an exhaustive study was performed by seismologists from around the world on all the major bridges in California. The Bay Bridge presents an unusual challenge

in seismic safety design because the West Span near San Francisco crosses deep shipping channels that are accessible to bedrock, whereas the East Span near Oakland crosses shallower waters filled with sediment. In order to seismically upgrade each portion of the Bay Bridge and maintain its functionality following a major seismic event, studies determined that the West Span, which was relatively undamaged during the Loma Prieta Earthquake, would require a seismic retrofit of the existing structure. The East Span, which sustained

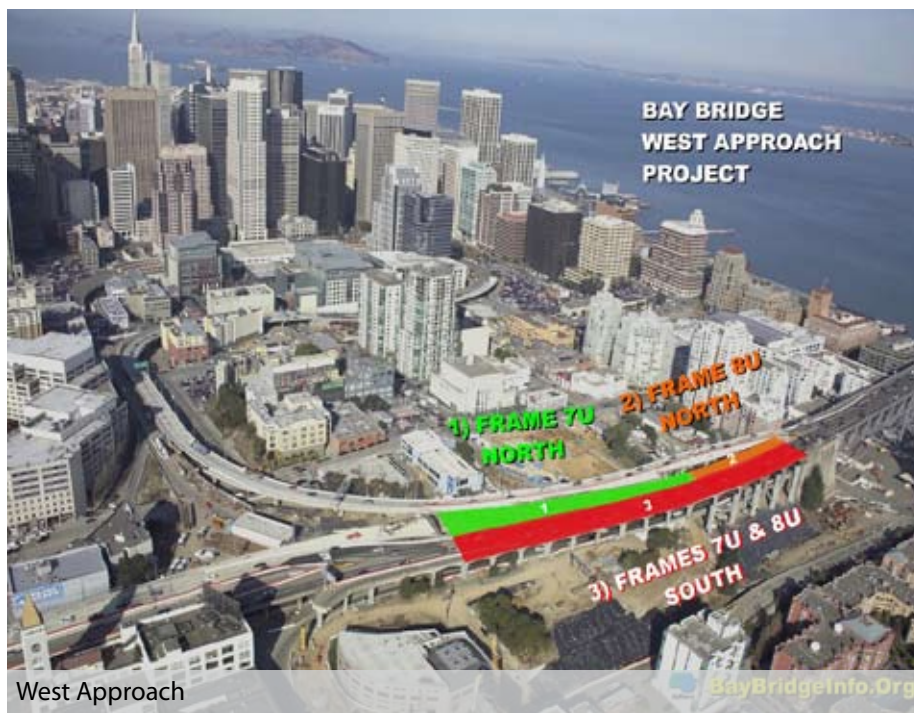
significant damage, would have to be entirely rebuilt. The one-mile West Approach to the bridge in San Francisco, which traverses the busy South of Market neighborhood, requires a retrofit-by-replacement.

Extensive seismic retrofit work has already been accomplished on several portions of the bridge, including the complete retrofit of the West Span suspension bridges. Other work is currently underway, throughout the corridor, in an elaborately choreographed effort to perform massive reconstruction and building with minimal impacts to daily traffic flow. Other projects are yet to begin. The entire bridge, including the signature Self-Anchored Suspension (SAS) span east of Yerba Buena Island, is slated for completion in 2013; followed by the demolition of the existing East Span.

### West Approach/West Span Retrofit

Work on the West Approach, a one-mile section of Interstate 80 in San Francisco between 5th Street and the San Francisco anchorage, involves completely demolishing existing structures and replacing them with seismically sound structures, within the footprint of the existing structure. The double-deck roadways from 5th Street to the anchorage will also be rebuilt so that each deck has an independent support column and foundation. Much of this work occurs within feet – and sometimes even inches – of residential and office buildings. To keep traffic flowing, the work requires elaborate staging: a temporary structure is built and vehicles are then rerouted. The old structure is then removed, and work begins on the new structure. When the new structure has been completed, vehicles are rerouted and the old structure is demolished. The replacement project is ongoing, and is scheduled to be complete in 2009.

An extensive retrofit of the West Span,





# 2006 YEAR IN REVIEW

January



First of two steel bridge sections, connecting the Skyway and SAS, arrived on site.

February



An eastbound steel bridge section weighing over 1,700 tons was lifted successfully. This was the heaviest lift ever completed by Caltrans.

March



Stormwater treatment measures contract was awarded.

April

Awarded the SAS Superstructure contract to American Bridge/Flour.



May



The last segment of the eastbound Skyway was lifted into place.

June

Demolition of a West Approach segment required weekend nighttime closures of the Bay Bridge.



The last segment of the Skyway was cast at the Stockton Pre-Cast Yard.



July

One year anniversary of the passage of AB 144 and the creation of TBPOC.



TOLL BRIDGE PROGRAM  
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August

A westbound steel bridge section weighing over 1,700 tons was lifted successfully. This piece will connect the Skyway and the SAS.



September

A 1,000-foot section of the West Approach's upper deck was demolished over Labor Day Weekend. The lower deck (eastbound) was closed to traffic.



October

First footing box for one of the two marine foundations for the SAS was completed.



November

Sixty-foot-long hinge pipe beams, which will connect the Skyway to the SAS, were installed for additional seismic safety.



December

The last two of 452 Skyway segments were installed. The segments averaged 85 feet wide, 25 feet long, and three stories tall.



Courtesy of Department of Transportation.

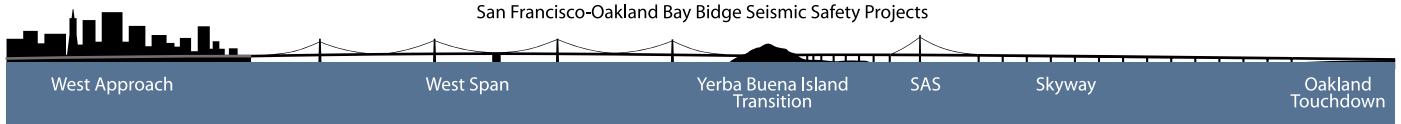
Figure 3: 2006 Year In Review



TOLL BRIDGE PROGRAM  
OVERSIGHT COMMITTEE

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which reaches from the San Francisco to the Yerba Buena Island (YBI) anchorages, was completed in 2004. Seismic-safety work on the span entailed a five-year effort to strengthen each section of the double-deck twin suspension spans.

### West Approach: A Labor Day Weekend Success Story

The West Approach successfully completed two major phases of demolition during 2006 – the larger of which occurred over Labor Day weekend, with the demolition of a 1,000-foot section of steel and concrete on the approach's upper deck near the San Francisco anchorage. To help ensure public safety, this monumental task required the closure of the bridge's lower deck for nearly 77 hours during the holiday weekend.

From a command center in San Francisco, the West Approach team worked closely with representatives from public transit and emergency service agencies. Many other agencies, including the California Highway Patrol, Bay Area Rapid Transit (BART), ferry and bus services and 511, City and County of San Francisco (CCSF) Department of Park-



West Approach

ing and Traffic, San Francisco Police Department, and numerous other CCSF agencies coordinated efforts to keep traffic flowing safely during this eventful weekend.

A major challenge was providing 24-hour public transit access to the lower deck on a limited basis throughout the weekend. Caltrans developed a plan with the San Francisco Municipal Railway (MUNI), AC Transit, and other tran-

sit agencies which required that a path for eastbound public transit vehicles was cleared each hour through most of the weekend.

The Labor Day closure required the most extensive public outreach performed to date on the Bay Bridge Project, as well as extensive coordination of public transit and the cooperation of numerous regional and local agencies. Numerous agencies coordinated efforts to disseminate information about the closures to a wide array of stakeholders, locally and throughout the state. MTC/511 provided trip planning updates on its website. BART and ferry boats, which provided expanded service during the closure, distributed fact sheets, ran electronic messages, and provided website updates. Information was also regularly updated through the official Bay Bridge website: [www.baybridgeinfo.org](http://www.baybridgeinfo.org). MUNI featured posters on more than a thousand vehicles, and other transit agencies also provided information to their riders. This intensive informational campaign proved successful in keeping the public fully informed of the upcoming bridge closure.



Demolition of West Approach structural frames over Labor Day Weekend







### East Span Replacement

The new east span will appear as a single unified span although it consists of several different structures. The graceful profile of the structure is revealed as a sleek and elegant white line which spans between Yerba Buena Island, and the Oakland shore. To further enhance its aesthetic appeal, the bridge will be lighted with a procession of roadway and tower lights that will provide a unique nighttime experience for both motorists and distant viewers.

The new span will feature the world's longest Self-Anchored Suspension Span (SAS), connected to the bridge decks (Skyway), which will gradually descend towards the Oakland shoreline (Oakland Touchdown). The east- and westbound lanes of the East Span will be reconfigured as side-by-side, thereby providing motorists more expansive views of the Bay Area. The new alignment allows traffic to continue flowing on the existing bridge as the new span is built. The new Yerba Buena Island (YBI) Transition Structure will connect the SAS to the YBI tunnel, facilitating the transition of side-by-side traffic from the SAS to the upper and lower decks of the YBI tunnel and the West Span.

The new East Span will provide five lanes of traffic and two shoulders in each direction of travel. On the south side of the eastbound deck, a bicycle/pedestrian pathway will be constructed one foot below the roadway, and will be separated from traffic such that it appears to "float" in the sky. The bicycle/pedestrian pathway will extend from the Oakland Touchdown to the western terminus of the East Span at YBI. The new East Span is scheduled to be complete in 2013.

### Self-Anchored Suspension Span

When completed, the Self-Anchored Suspension Span (SAS) will be the longest suspension bridge of its kind and the signature span of East Span. Its



New East Span Skyway under construction

single, elegant tower will reach 525 feet above sea level, complementing the highest tower on the bridge's West Span. The SAS has been designed to be both uniquely aesthetic and functional -- capable of withstanding a major earthquake. The single tower is composed of four separate legs connected by shear link beams, which are designed to move separately and to absorb seismic forces during an earthquake, preventing catastrophic damage to the main structure. Any dam-

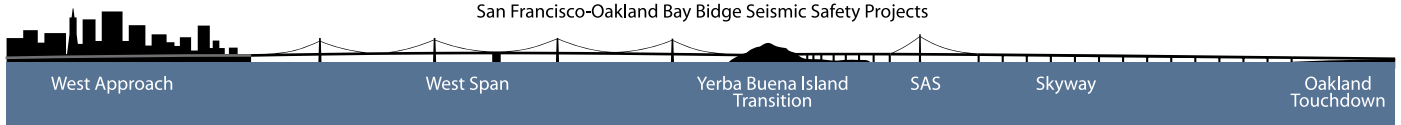
aged link beams can later be removed and replaced.

In August 2006, the SAS Superstructure received the "Best of What's New" Award from *Popular Science* magazine. It is the longest single tower, self-anchored suspension bridge in the world. Traditional main cable suspension bridges have twin cables with smaller suspender cables connected to them. These cables support the roadbed and are anchored to separate structures in



Self-Anchored Suspension Span rendering





Stockton Pre-Cast Yard

the ground. By contrast, there is only one main cable on the new SAS span; it will be anchored to the deck on the east end and looped around the roadbed at the west end.

Work on the SAS foundation was started in late 2005, and includes the construction of water-based and land-based foundations. The land-based foundation on YBI is already complete. The SAS foundation work is on schedule to finish on or before March 2008.

On April 18, 2006 -- the centennial anniversary of the 1906 earthquake in San Francisco -- the SAS contract was awarded to American Bridge/Fluor Enterprises.

#### Skyway

The 1.2-mile Skyway section is located between the limits of the SAS span and the Oakland Touchdown. The two parallel Skyway superstructures consist of 452 prefabricated concrete segments, supported by piers. Large deck segments, with the average size of 25 feet long by 90 feet wide by 30 feet tall, were pre-cast at a yard in Stockton and transported by barge to the project site.

A major milestone was reached on December 8<sup>th</sup>, 2006 when the last of 452 concrete deck segments that comprise the new Skyway was lifted into place. On average, these segments weigh over 700 tons and are the largest of their kind in the world. A total of 24 piers support the 452 concrete roadway segments that make up these twin parallel bridges that will carry both directions of traffic.

Another major milestone on the Skyway in 2006 was the lifting of enormous steel segments that will connect the Skyway and the SAS. It was the heaviest lift Caltrans performed in its history. The steel segments weigh over 1,700 tons were lifted successfully in February and August 2006.

#### Yerba Buena Island

The Yerba Buena Island Transition Structure (YBITS) will connect the YBI tunnel and the East Span. The YBITS will begin on the island, and allow traffic movement between the double-decked structure and the two side-by-side bridge structures of the east span.

The YBI South South Detour (SSD) is a temporary detour structure parallel to the existing bridge. Detouring traffic onto the SSD will be a major event requiring expanded public information and outreach efforts, which will be modeled after the successful West Approach campaigns. While traffic is detoured onto the SSD, the permanent transition structure, YBITS, will be constructed. The SSD is currently under construction.



Yerba Buena Island W2 Pier







### Oakland Touchdown

The Oakland Touchdown (OTD) will be located in Alameda County on Interstate 80 west of the toll plaza. It is the structure that will connect to the East Span Skyway. The OTD construction has been divided into three major contracts, in order to coordinate with other major work on the East Span. The three construction contracts are: 1) OTD Submarine Electrical Cable Relocation, 2) OTD #1 (construction of all marine foundation and the westbound bridge section), and 3) OTD #2 (the eastbound bridge section).



Oakland Touchdown rendering

Design work for the OTD #1 is complete, and plans, specifications, and engineer's estimate (PS&E) were submitted to the Caltrans Office Engineer in September 2006. The advertisement for this contract is scheduled for early 2007, and the contract completion is scheduled for late 2009.

The OTD #2 contract will occur once the westbound traffic is shifted onto the new SAS. The OTD #2 contract will be advertised in 2010 in time for the SAS opening.

### Other Bay Bridge Highlights

#### Neighborhood Outreach

The Bay Bridge Seismic Safety Projects, especially work on the West Approach



Media outreach

and Yerba Buena Island, affect project neighbors. Consequently, special efforts are made to keep these neighbors informed about upcoming work.

Neighborhood and broader public meetings are scheduled as needed to discuss construction activities and to hear from local residents and business owners. Updated information on bridge construction affecting project neighbors can be viewed on the project website: [www.baybridgeinfo.org](http://www.baybridgeinfo.org). The website provides information on construction activities and roadway closures. In total, hundreds of public meetings have been held, numerous phone inquiries have been responded to, and public announcements and other information have been disseminated throughout the Bay Area and beyond.

The Labor Day weekend closures required the largest neighborhood outreach to date and extended well beyond the immediate project vicinity. A team of over 50 youths from community-based organizations helped to distribute nearly one million fact sheets about the closures to project neighbors, as well as to hotels and hospitals, taxi and shuttle services, chambers of commerce, and tourism offices throughout the Bay Area. They also canvassed regional airports.

#### Bay Bridge Public Information

In 2005, the TBPOC approved an extensive Communications Plan to guide

community outreach activities forward. This plan describes key methods and processes for minimizing potential disruption to motorists and the general public during construction, and to keep the public, motorists, local government, transit agencies, residents and businesses informed of major construction activities.

Located at the foot of the bridge at Pier 7 in Oakland, the Public Information Office reached out to a wide array of stakeholders during the past year through several monumental public outreach campaigns. The Office also organized several contractor outreach



meetings during the advertisement of the SAS and the stormwater treatment contracts; provided numerous construction site tours and presentations; and established a media archive to chronicle the coverage of this historic work.

In 2006, the Bay Bridge Public Information Office launched the following communications tool:

- A new definitive website, providing up-to-date information for all of the Bay Bridge Projects: [www.baybridgeinfo.org](http://www.baybridgeinfo.org)
- A new newsletter, *Bay Bridge News*, distributed to over 5,000 subscribers in print and electronically, covers major project milestones





- E-Alerts, to provide timely information regarding upcoming major construction activities.

Media outreach is a key component of the Communications Plan. In 2006, the Bay Bridge was favorably featured in numerous media outlets; the PIO team also provided several media outreach events to mark major project milestones, including the SAS bid opening and award; the pre-cast segment lifts on the Skyway; and the completion of segment fabrication at Stockton Yard. The Office assisted in the creation of several national documentaries and feature-length programs produced by the History Channel, National Geographic, Discovery, CBS, and the Science Channel, to name a few.

For its outstanding work, the Bay Bridge Public Information Office (PIO) was honored with several noteworthy

awards in the past year, including the American Association of State Highway and Transportation Officials award for Best National Print/Electronic Publications Media Kit and the Metropolitan Transportation Commission's "Excellence In Motion" award, recognizing the Office's extensive public outreach efforts during the lower deck closures over Labor Day weekend on the West Approach.

#### Context Sensitive Design

Context-Sensitive Design (CSD) is a process for achieving design excellence by developing transportation solutions that require continuous, collaborative communication and consensus-building between transportation agencies, professionals, and stakeholders. A common goal of this process is to develop a facility that is harmonious with the community, and preserves aesthetics, history and environmental resources,

while integrating these innovative approaches with traditional transportation goals for safety and performance.

Located in the magnificent Bay Area landscape, the East Span warrants landmark attention. This new East Span will serve as "a bridge of the 21st century" that will take its rightful place among the neighboring West Span, and the Golden Gate Bridge, serving as a gateway to Oakland and the East Bay. The visual success of this structure is due in part to the goals of CSD policy. The cities of Oakland and San Francisco, as well as the Design Review Board (DRB) of the San Francisco Bay Conservation and Development Commission (BCDC), and the local Bicycle Coalition, were vital in this effort. They have worked closely with Caltrans and their design consultants, as well as many other agencies, and will have had a lasting effect on the overall appearance and de-



East Span Bicycle/Pedestrian Pathway







sign of the structure, as well as the environmental mitigations and protections implemented as part of the project.

Much of the progress on the bridge for 2006 was influenced by past and current design with stakeholders, and is evident in the contract documents (plans and specifications), construction practices, and completed portions of the SAS and Skyway structures. The architectural form and detail of the SAS, Skyway, piers, and bicycle/pedestrian pathway, are all a result of this collaborative process. The fact that the SAS and bicycle/pedestrian pathway are a part of the project today is truly a triumph in CSD, in that it was the community that rallied and supported these amenities. Further, efforts to minimize adverse impacts to the environment have been a continued priority of this project, and have been implemented as part of the construction process to restore tidal habitat, limit fish mortality, and enhance bird habitat.

#### Environmental Considerations

The project team is committed to completing the project in an environmentally friendly manner by using innovative techniques where appropriate. Biological mitigation and monitoring are being implemented in accordance with the requirements of the Federal Highway Administration and the various permitting agencies. Biologists have been regularly monitoring water quality for turbidity, as well as local species of birds, fish, and marine mammals. All weekly, monthly, and annual compliance reports to resource agencies have been delivered on time.

Caltrans is also working with multiple agencies to develop off-site mitigation opportunities for creation or improvement of habitat in the north and central Bay. The one-year eelgrass pilot program at the North Basin site was completed in July 2006. The monitor-

ing of eelgrass beds will be continued for another year.

Other environmental highlights of 2006 include:

- Installation of cormorant platforms between the two parallel Skyway structures, which will provide alternative nesting locations for cormorants nesting on the existing bridge.
- Protection of marine habitat by using a curtain of air bubbles to minimize sound waves in the water caused by pile driving.
- Construction of stormwater treatment measures, including bioretention basins to treat runoff from the bridge and nearby roadways before it enters the Bay. The work complies with the California Regional Water Quality Control's Discharge Requirement.

#### Project Schedule

The Bay Bridge project is large and complex consisting of multiple contracts. The interdependencies among the major projects are numerous, as

shown in Appendix B. The Bay Bridge project requires careful monitoring and coordination to assure that construction will be completed on schedule.

A Corridor Schedule Team (CST) has been established whose primary function is to identify and mitigate corridor schedule risks. The CST integrates and coordinates schedules with the project schedule teams, reviews opportunities to enhance the corridor schedule, and provides recommendations to program management regarding schedule decisions and risk mitigation. The CST has helped further the Caltrans' goal of completing the Toll Bridge Program expeditiously by providing recommendation on the contract award for the YBITS.

As of December 2006, the East Span is scheduled to be open to traffic in 2012 in the westbound direction and in 2013 in the eastbound direction. The work sequences of the Bay Bridge West Approach and East Span are provided in Figures 4 and 5, respectively.



Stormwater pump station



# SFOBB West Approach Work Sequence

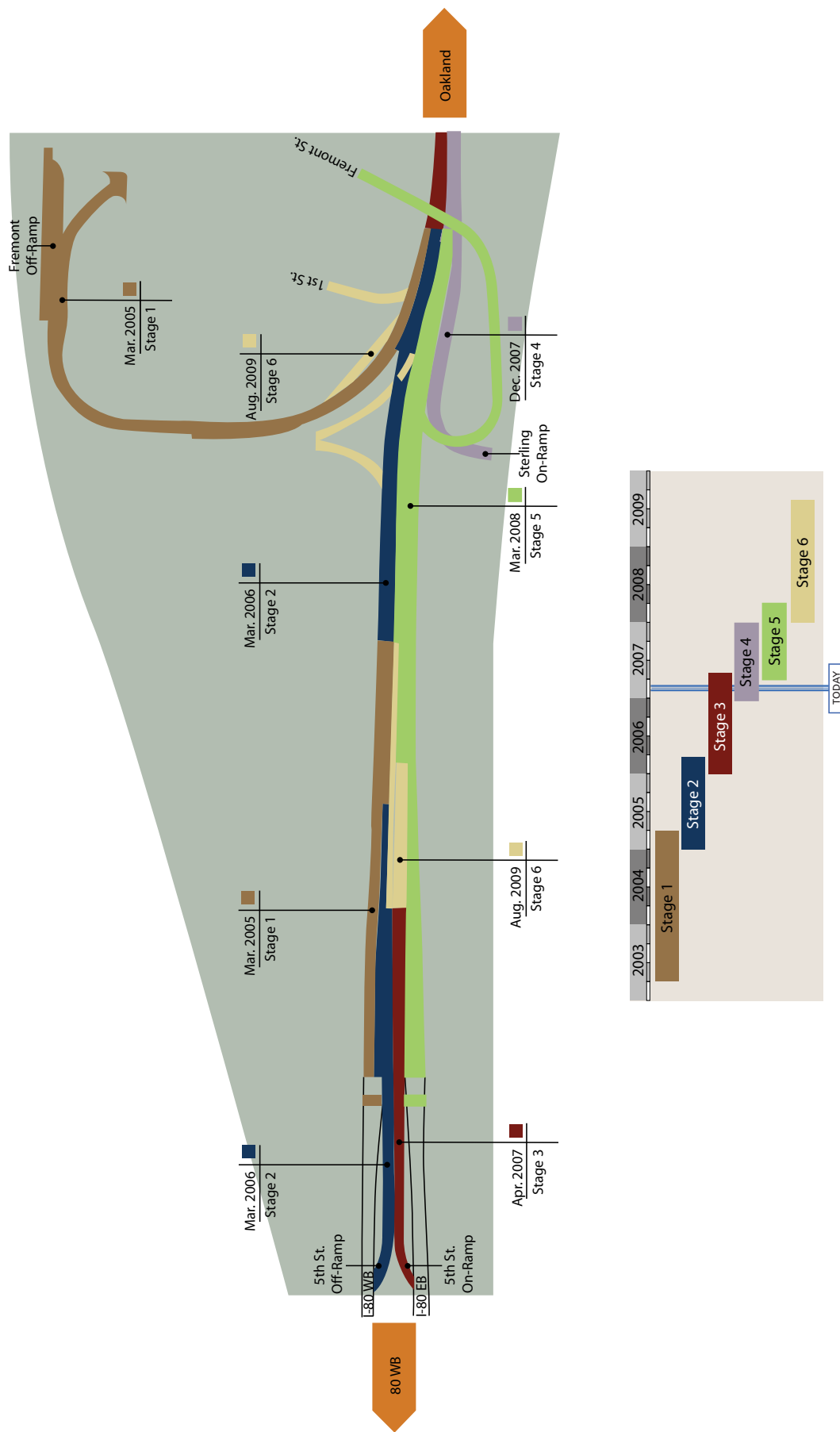


Figure 4: West Approach Schedule

Note: Dates shown above are project completion dates.  
Source: Caltrans, January 2007

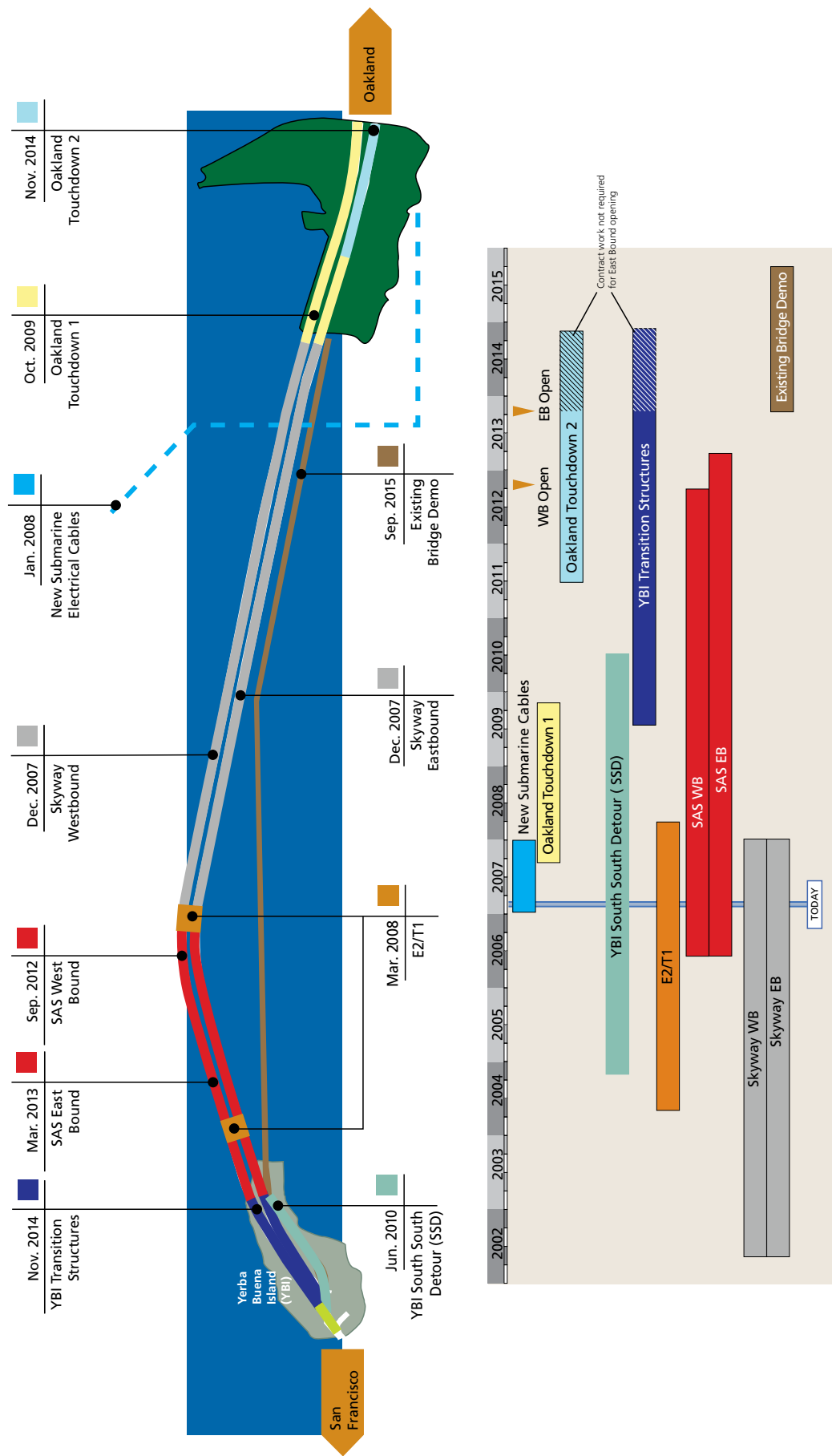


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2007 LEGISLATIVE UPDATE  
DRAFT



# SFOBB East Span Work Sequence



Note: Dates shown above are project completion dates.  
Source: Caltrans, January 2007

Figure 5: East Span Schedule



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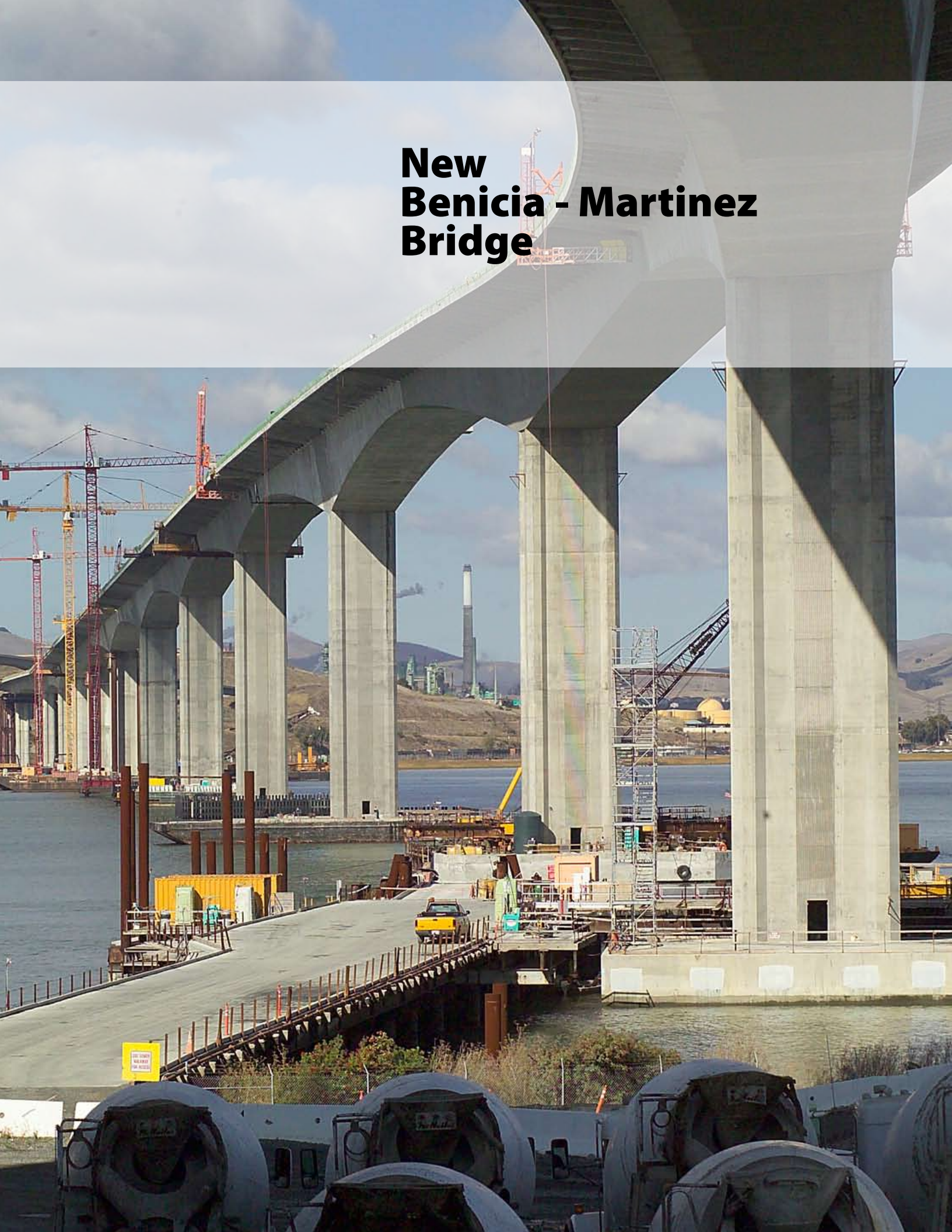
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# New Benicia - Martinez Bridge







Existing Benicia - Martinez Bridge, Union Pacific Railroad, New Benicia - Martinez Bridge (from right to left)

The existing Benicia-Martinez Bridge provides access across the Carquinez Strait between Contra Costa and Solano counties via Interstate 680 and carries 117,000 vehicles per day. A new span that will carry five lanes of northbound traffic is being built just east of and parallel to the existing span. The existing span will be converted to carry four lanes of southbound traffic, as well as a new bicycle /pedestrian pathway. As part of the new bridge project, a new 12-lane toll plaza with one carpool bypass and two FasTrak® express lanes will be constructed. The new bridge project also includes the reconstruction of the I-680/Marina Vista Road and I-680/I-780 interchanges.

Significant progress was made on the new bridge project in 2006: the I-680/Marina Vista Interchange Reconstruc-

## New Benicia - Martinez Bridge

### 2006 Highlights



tion was completed; the I-680/I-780 Interchange was structurally completed; and, the last segment of the bridge was poured.

The new bridge is scheduled to be open to traffic by December 2007. All construction projects are scheduled to be complete in December 2009.

The current approved budget for the bridge is \$1.3 billion, and the project is forecasted to be complete within the approved budget.



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# **Richmond - San Rafael Bridge**







Richmond - San Rafael Bridge

The Richmond-San Rafael Bridge provides an important connection between Marin County and the cities of the East Bay via Interstate 580, carrying 80,000 vehicles per day. The Richmond-San Rafael Bridge seismic retrofit project was completed in 2005, \$89 million under the approved total budget of \$914 million. In October of 2006, the TBPOC authorized the transfer of these cost savings to the Toll Bridge Program's contingency.

One last remaining portion of work for the bridge includes the Richmond-San Rafael Public Access Project, which will provide public access to the Bay shoreline at the west end of the bridge in Marin County. The project includes a new sidewalk bus-stop landing, a parking area for six cars, a ten-foot wide

## Richmond - San Rafael Bridge

### 2006 Highlights



shoreline trail, pedestrian bridge, picnic tables and benches. New rock slope protection will be placed at the bay-shore to protect against erosion, and drought tolerant landscaping will be planted to enhance the overall appearance of the project. The project will also protect salt marsh wetlands. The project will allow immediate access to the shoreline for motorists, bicyclists, and pedestrians to enjoy walking, picnicking, fishing, and the picturesque views across the Bay. This project was awarded in November 2006. Construction of the project began in January 2007, and is expected to be complete in summer 2007.





Larkspur

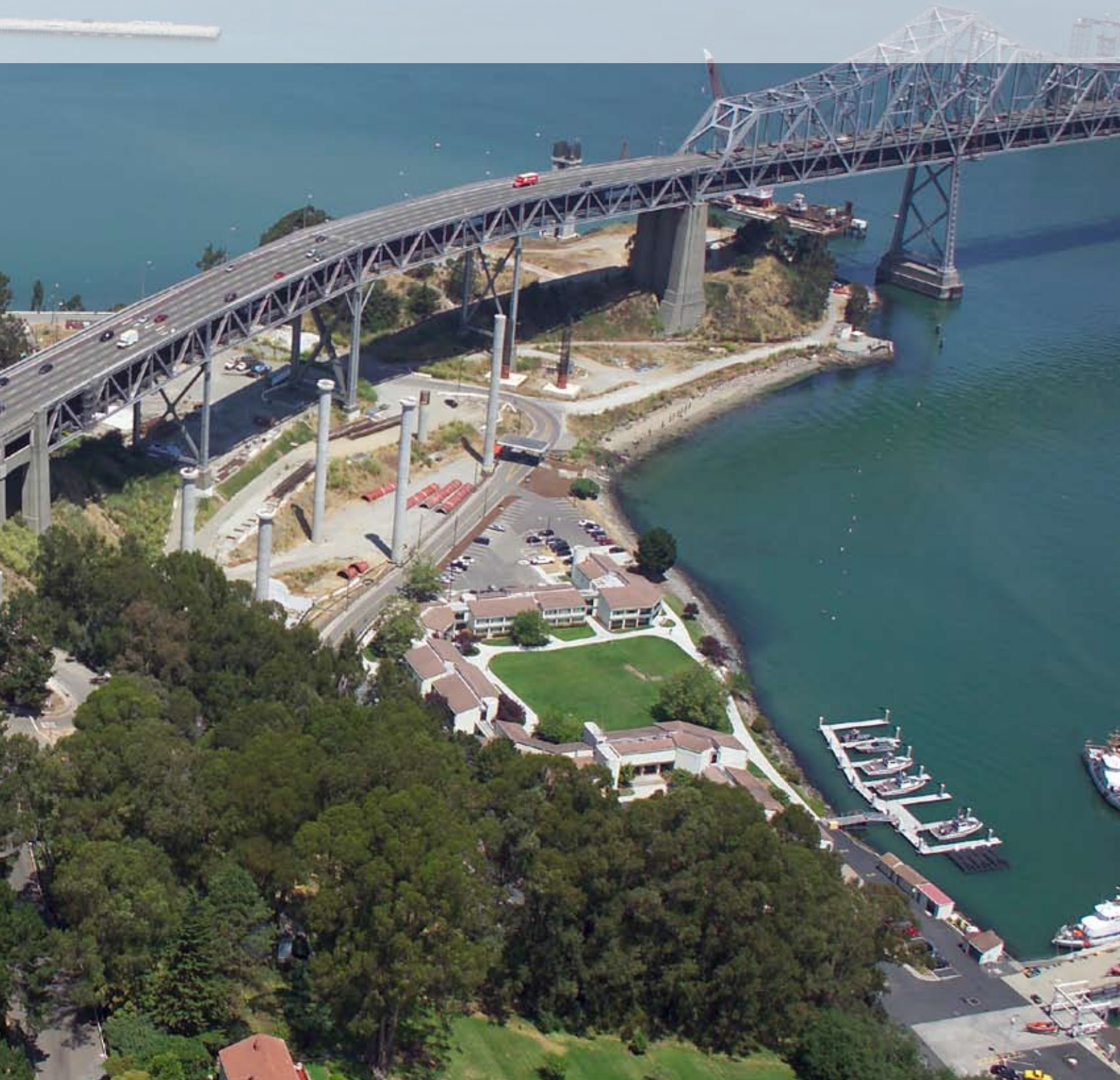
San Rafael

Richmond-San Rafael Bridge Public Access Project





# 2007 Look Ahead







W3 Columns on Yerba Buena Island

The year 2007 presents new challenges as well as several new milestones. The TBPOC continues to oversee work towards completing the series of Bay Bridge Seismic Safety Projects. The Risk Management and Corridor Schedule Teams will continue their coordination to identify and address schedule and cost impacts to keep projects on schedule. Many of the upcoming projects will require significant public outreach, as they impact traffic and nearby residents and businesses.

The following section provides a snapshot of what to expect this year.

### **SAN FRANCISCO - OAKLAND BAY BRIDGE** Project Activities

A multitude of diverse construction re-

## **2007 Look Ahead**

- *SAS Marine Foundation Completion*
- *Major Traffic Shift onto West Approach Temporary Eastbound Bypass*
- *East Span West Tie-In over Three-Day Closure*
- *Skyway Completion*
- *Benicia-Martinez Bridge Completion*

lated activities for the Bay Bridge will continue in the year 2007. Major activities will include commencement on the SAS Superstructure, YBI South South Detour, and West Tie-In projects. The

OTD Submarine Electrical Cable Relocation contract was awarded in January 2007 and is scheduled to be complete by year's end. The Oakland Touchdown #1 Contract will be advertised in early 2007, and construction will begin in late 2007. The stormwater treatment project is scheduled to be complete this year, as well as the Skyway contract, marking major milestones for 2007.

In spring 2007, the eastbound traffic on the West Approach will be detoured onto a temporary structure for approximately one year. The switch is planned to be carried out over weekend night hours, thereby minimizing potential traffic disruption. While traffic is de-





toured onto the temporary structure, the remaining mainline structure will be demolished and replaced.

The first phase of the West Tie-In (WTI) is a retrofit-by-replacement for a 300-foot segment of the western most part of the existing East Span. Completion of the WTI Phase 1 requires a full bridge closure over a three-day weekend to allow for the removal and replacement of the 300-foot upper deck segment. The WTI Phase 1 is scheduled to be complete in 2007. The closure will be modeled after the successful public outreach and transit agency coordination executed for the Labor Day Weekend 2006 closure. The weekend closure is a key component towards completion of the South South Detour, which will accommodate traffic while the YBITS is built.

The year 2007 will also bring the completion of the water-based foundations for the SAS, and the final touches and completion ceremony for the Skyway. Subsequently, the Skyway will be used as a staging area for construction of the SAS.

#### 2007 Highlights:

- *Advertise and award Oakland Touchdown Contract #1*
- *Complete construction of Skyway, including bicycle/ pedestrian pathway, viewing platforms, lighting and railing*
- *Continue environmental monitoring programs and complete construction of the storm-water treatment measures*

#### Public Outreach

The Public Information Office team will continue to implement the approved Communications Plan, which guides community outreach activities. Major activities for 2007 include:

- Wide-scale public outreach campaign to support the three-day full bridge closure necessary for the first phase of the Yerba Buena Island West Tie-In

replacement on the South South Detour;

- Media and neighborhood outreach campaign during the West Approach eastbound traffic re-alignment and deck demolition scheduled to occur in the spring of 2007;
- Media coverage and events, as appropriate, for the completion of the Skyway and E2/T1 marine foundations, and construction of the SAS superstructure;
- Regular updates about the ongoing Bay Bridge Seismic Safety Projects, including the SAS, the Storm Water Treatment Project, and the Oakland Touchdown;
- Establishment of a Public Information Office on Treasure Island and continued development of a partnership with the Treasure Island Development Authority; continued development of the Pier 7 Visitor Center; and, continued tours and presentations;
- Ongoing updates about the Bay Bridge Seismic Safety Projects to media, industry, local communities, motorists and the general public, including the publication of outreach materials, such as the Bay Bridge News and E-Alerts.

#### BENICIA-MARTINEZ BRIDGE

After five years of construction, the new Benicia-Martinez Bridge is scheduled to be open to traffic by December 2007

#### RICHMOND-SAN RAFAEL BRIDGE

Construction of the Public Access Project began in January 2007 and is scheduled to be complete in summer 2007.

#### TOLL BRIDGE PROGRAM

##### Small Business Program

In December 2006, the Toll Bridge Program embarked on the development of the Small Business Program. This unique training and outreach program is being designed to enhance small business expertise, assist with resource identification, grow existing businesses, and identify opportunities for contract work on the Bay Bridge projects.

The program will offer a series of professional development courses that will be taught by industry experts, and address a range of topics ranging from marketing strategies to-business systems to-cutting edge technologies, that will provide real world expertise and the competitive edge for small and emerging businesses in the market place.

The Small Business Education and Training Program will experience a full program ramp-up in spring 2007.



New Benicia-Martinez Bridge scheduled to open in 2007







## Appendix



# Appendix A

**Table 1: Toll Bridge Program Funding**  
(as of December 31, 2006)

|  | Budgeted        | Funding<br>Available &<br>Contributions |
|--|-----------------|---|
| Toll Financing   | (In \$Millions) |   |
| Seismic Surcharge Revenue AB 1171                              | \$ 2,282.0      | \$ 2,282.0                              |
| Seismic Surcharge Revenue AB 144                               | \$ 2,150.0      | \$ 2,150.0                              |
| BATA Consolidation   | \$ 820.0        | \$ 820.0                                |
| Subtotal - Financing   | \$ 5,252.0      | \$ 5,252.0                              |
| Direct Contribution  |                 |   |
| Proposition 192  | \$ 790.0        | \$ 789.0                                |
| San Diego Coronado Toll Bridge<br>Revenue Fund                 | \$ 33.0         | \$ 33.0                                 |
| Vincent Thomas Bridge  | \$ 15.0         | \$ 6.9                                  |
| State Highway Account  | \$ 745.0        | \$ 745.0                                |
| Public Transportation Account                                  | \$ 130.0        | \$ 90.0                                 |
| ITIP/SHOPP/Federal Contingency                                 | \$ 448.0        | -                                       |
| Federal Highway Bridge<br>Replacement and Rehabilitation (HBI) | \$ 642.0        | \$ 500.0                                |
| SHA – East Span Demolition                                     | \$ 300.0        | -                                       |
| SHA – “Efficiency Savings”                                     | \$ 130.0        | \$ 2.0                                  |
| Redirect Spillover   | \$ 125.0        | -                                       |
| Motor Vehicle Account  | \$ 75.0         | \$ 75.0                                 |
| Subtotal - Contributions                                       | \$ 3,433.0      | \$ 2,240.9                              |
| Total Funding  | \$ 8,685.0      | \$ 7,492.9                              |
| Allocated to Date  |                 | \$ 6,013.3                              |
| Remaining Unallocated  |                 | \$ 1,479.6                              |

Source: Toll Bridge Seismic Retrofit Program Fourth Quarter Report, as of December 31, 2006.  
Toll Bridge Program Oversight Committee.



**Table 2: Toll Bridge Program Approved Budget**  
( as of December 31, 2006)

| Contracts                   | Current Approved Budget<br>(\$ Million) |
|-----------------------------|---|
| <b>Completed Projects</b>   |   |
| Benicia-Martinez            | \$177.8                                 |
| Carquinez                   | \$114.2                                 |
| San Mateo-Hayward           | \$163.5                                 |
| Vincent Thomas              | \$58.5                                  |
| San Diego-Coronado          | \$103.5                                 |
| SFOBB West Span             | \$307.9                                 |
| <b>Ongoing Projects</b>     |   |
| Richmond-San Rafael         | \$825.0                                 |
| SFOBB West Approach         | \$429.0                                 |
| SFOBB East Span             | \$5,486.6                               |
| Miscellaneous Program Costs | \$30.0                                  |
| Subtotal                    | \$7,696.0                               |
| Program Contingency         | \$989.0                                 |
| <b>Total Program</b>        | <b>\$8,685.0</b>                        |

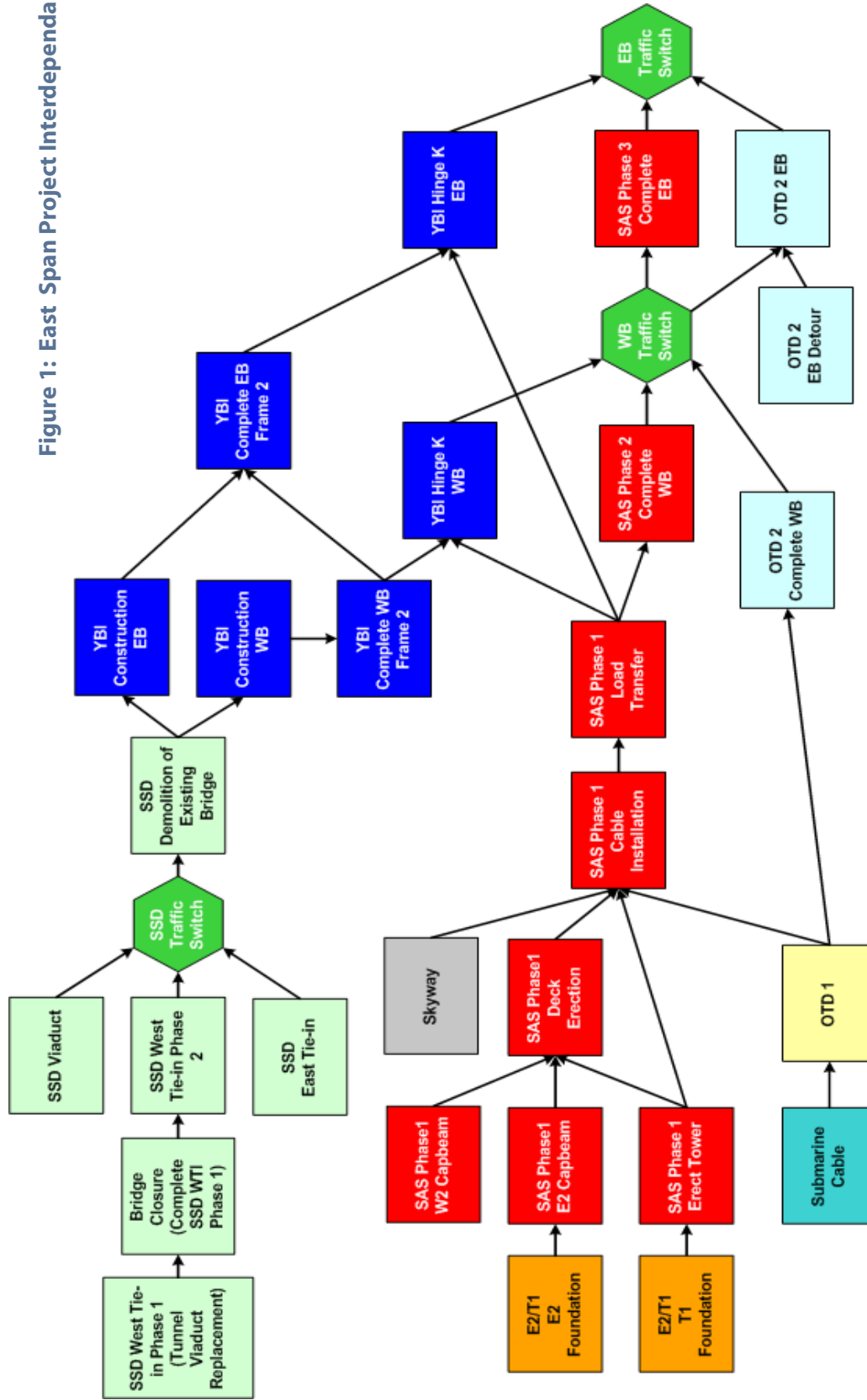
Source: Toll Bridge Seismic Retrofit Program Fourth Quarter Report, as of December 31, 2006.  
Toll Bridge Program Oversight Committee.





# Appendix B

Figure 1: East Span Project Interdependency



**Photographers:**

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Maura Twomey  
Jason Weinstein







# TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

## *Memorandum*

**TO:** Toll Bridge Program Oversight Committee (TBPOC)      **DATE:** February 2, 2007

**FR:** Tony Anziano, Caltrans, Program Manager

**RE:** Agenda No. - 2  
Final Draft Fourth Quarter Report  
Item- Ending December 31, 2006

---

**Cost:**

N/A

**Schedule Impacts:**

N/A

**Recommendation:**

Approval

**Discussion:**

Attached, for your information, is the 4<sup>th</sup> Quarter 2006 Report Schedule which reflects the status of completed report tasks and the schedule for remaining actions.

For your approval is the Final Draft Fourth Quarter Report Ending December 31, 2006 e-mailed to you on February 2<sup>nd</sup>. The fourth quarter forecast is being refined and will be provided as soon as available.

**Attachment:**

4<sup>th</sup> Quarter 2006 Report Schedule





# TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

| Action  | Deadline for Action          |
|---|------------------------------|
| <b>4th Quarter 2006 Report: Legislated Deadline - February 14, 2007</b>                 |                              |
| BAMC Begin Quarterly Report Development; Issue First Call for Input                     | Monday, December 18, 2006    |
| BAMC Prepare Quarterly Report 1st Draft for PMT, BATA, Caltrans                         | Monday, January 08, 2007     |
| PMT / BATA / Caltrans Review & Comment on 1st Draft                                     | Thursday, January 11, 2007   |
| BAMC Incorporate Comments: Produce 2nd Draft for TBPOC Review                           | Friday, January 12, 2007     |
| TBPOC Review & Comment on 2nd Draft   | Monday, January 15, 2007     |
| Expenditure Update (Anticipated Date)   | Monday, January 22, 2007     |
| BAMC Incorporate Comments; Produce Proposed Final Draft for TBPOC and Agency            | Tuesday, January 23, 2007    |
| BAMC Issue Proposed Final Draft to TBPOC & Agency                                       | Thursday, January 25, 2007   |
| TBPOC and Agency Review / Comment on Proposed Final Draft                               | Friday, February 02, 2007    |
| BAMC Incorporate Comments: Produce Advanced Final Draft + Table of Conflicting Comments | Wednesday, February 07, 2007 |
| TBPOC Teleconference to make Final Comments and Resolve Conflicting Comments            | Friday, February 09, 2007    |
| BAMC Incorporate All Final Comments from TBPOC; Emails Final Version for Information    | Monday, February 12, 2007    |
| Produce & Issue Quarterly Report to Legislature & CTC                                   | Tuesday, February 13, 2007   |